

**WARD:** Hale Central

**84747/FUL/15**

**DEPARTURE: No**

**Change of use to mixed use retail and restaurant; works to include erection of single storey side/rear extension; replacement shop front; erection of 3xno. air conditioning units to the rear; provision of bin store to rear and ancillary works thereto.**

166 Ashley Road, Hale, WA15 9SF

**APPLICANT:** Evuna Restaurants

**AGENT:** Pearson Architects

**RECOMMENDATION: GRANT**

---

**Councillor Mrs Young has requested that this application be determined at Planning Development Control Committee for the reasons set out in the report.**

### **SITE**

The application site relates to retail unit within a commercial terrace fronting onto Ashley Road within the designated Hale District Centre area, Cecil Road extends along to the rear of the building. The premises are on the south-west side of Ashley Road and had previously been used as a florists but are currently vacant. The premises comprises a small basement area, with the main accommodation over the ground and first floor areas, a second floor area within the building is accessible but has restricted headroom to the stairwell access so it would not be suitable for public access.

The first floor area has been used as an office (B1/A2), access to the office is from a separate entrance from Cecil Road. The site is not currently within a Conservation Area; the boundary of Hale Station Conservation Area extends along and includes 158-160 Ashley Road and extends in a northerly direction. There are no listed buildings immediately surrounding the application site. The application site falls within a main shopping frontage as identified within Revised Trafford UDP (Appendix H)

### **PROPOSAL**

This application seeks planning permission for a change of use of both ground and first floor to A3 (Restaurant), the applicant has stated the restaurant would concentrate mainly on Spanish cuisine. In line with this the applicant has stated that an important aspect of their business comprises the retail of Spanish wine, speciality meats, olive oils and related products and therefore the application description refers to a retail element as well as the A3 use.

Works are also proposed to the fabric of the building and include a single storey rear extension; replacement shop frontage; provision of 3x air conditioning units to the rear of the building as well as a new bin store area also located to the rear of the site. The second floor area is proposed to be used as storage.

Proposed opening hours are Monday to Saturday 10.00am – 23.30pm and Sundays and Bank Holidays 10.00am – 22.00pm.

## **FLOORSPACE**

The total floorspace of the proposed extension would be approximately 7m<sup>2</sup>.

## **DEVELOPMENT PLAN**

### **The Development Plan in Trafford Comprises:**

- The **Trafford Core Strategy**, adopted 25th January 2012; The Trafford Core Strategy is the first of Trafford's Local Development Framework (LDF) development plan documents to be adopted by the Council; it partially supersedes the Revised Trafford Unitary Development Plan (UDP), see Appendix 5 of the Core Strategy.
- The **Revised Trafford Unitary Development Plan (UDP)**, adopted 19th June 2006; The majority of the policies contained in the Revised Trafford UDP were saved in either September 2007 or December 2008, in accordance with the Planning and Compulsory Purchase Act 2004 until such time that they are superseded by policies within the (LDF). Appendix 5 of the Trafford Core Strategy provides details as to how the Revised UDP is being replaced by Trafford LDF.
- The **Greater Manchester Joint Waste Plan**, adopted 1<sup>st</sup> April 2012 now forms part of the Development Plan in Trafford and will be used alongside district-specific planning documents for the purpose of determining planning applications.
- The **Greater Manchester Joint Minerals Plan**, adopted 26<sup>th</sup> April 2013 now forms part of the Development Plan in Trafford and will be used alongside district-specific planning documents for the purpose of determining planning applications.

### **PRINCIPAL RELEVANT CORE STRATEGY POLICIES**

L4 – Sustainable Transport and Accessibility

L7 – Design

W1 – Economy

W2 – Town Centre & Retail

### **PROPOSALS MAP NOTATION**

Hale Town and District Shopping Centre

Main Shopping Frontage (Hale 160-190 Ashley Road even)

### **PRINCIPAL RELEVANT REVISED UDP POLICIES/PROPOSALS**

S13 – Non Shop Service Uses within Town and District Shopping Centres.

## **NATIONAL PLANNING POLICY FRAMEWORK (NPPF)**

The DCLG published the National Planning Policy Framework (NPPF) on 27 March 2012. The NPPF will be referred to as appropriate in the report.

## **RELEVANT PLANNING HISTORY**

H/3388 – change of use from flat to office use (1<sup>st</sup> floor 166 Ashley Road) – Approved 20/05/1976

## **APPLICANT'S SUBMISSION**

The applicant has submitted the following information in support of the application which will be referred to as necessary within this report:-

- Planning Statement
- Plant Noise Assessment
- Statement regarding loss of A1 use.

## **CONSULTATIONS**

### **Pollution Housing and Licencing -**

#### Noise Control

The applicant has submitted an acoustic concern to address concerns raised by this section previously in relation to potential noise disturbance from heating and ventilation plant attached to the building.

The report concludes that attenuators and acoustic enclosures are required for each piece of plant to ensure that the agreed noise criteria is reached (at the nearest residential property). Each piece of plant will need to be attenuated to achieve a 10dB reduction.

In view of the acoustic report provided and the work that is required to achieve the noise criteria it is recommended the following conditions are included on any decision notice for this application:

- 1) Prior to the first use of the development applied for, the scheme of noise mitigation measures detailed in 'Evuna Hale, 166 Ashley Road, Hale- Plant Noise Assessment' dated 26 May 2015 shall be implemented and retained as such thereafter. Confirmation shall be provided in writing to the Planning Authority that suitable attenuators and enclosures have been installed on the external plant.
- 2) Refuse shall only be collected from the premises between the hours of (7:30am-21:00 Mon-Fri, 09:00-21:00 Sat and no time on Sunday's or Bank Holidays)

where such disposal or collection is likely to cause disturbance to local residents.  
Reason: In accordance with L5.13 of the Trafford Core Strategy.

### Odour Control

- 1) Details of the fume extraction system serving the cooking or/and food preparation areas shall be designed such that there will be no odour nuisance to residential premises and shall be approved by the Local Planning Authority prior to the development taking place. Reason: In accordance with L5.13 of the Trafford Core Strategy.

**Local Highway Authority (LHA)** – No objections, the applicant to provide 2 secure cycle parking spaces for staff use.

**Drainage (Trafford Council)** – No objections, subject to standard informative preventing discharge of grease, fats or solid foods to the public sewerage system.

### REPRESENTATIONS

**Councillor Mrs Young** has objected to the proposed development with regards noise, traffic issues and impact on amenity of neighbours and has requested that the application be determined at planning committee.

**Councillor Candish** has also raised an objection to the proposal with regards noise, traffic issues and particularly with regards loss of amenity.

**Neighbours:-** Letters of objection to the proposed development have been received from 7 individual addresses citing the following concerns:-

### Amenity

- First floor kitchen and dining area will result in noise and disturbance from the activities associated with such use to the adjacent office use at 168 Ashley Rd (Now Financial Planning). Meetings with clients and conducting business will be made impossible by the disturbance.
- 3x air conditioning units will also result in noise and disturbance
- Pollution and smells from the restaurant will impact on the use of the neighbouring offices.
- Vermin and smells from the bin area
- Inadequate insulation between No. 166 Ashley Rd and 168 Ashley Rd will exacerbate noise
- Noise from commercial vehicles, Cecil Rd is a residential road.
- Bin area should be within the building and have a roof over to conceal it
- Extract flue and vent is not situated above ridge line therefore odours will enter neighbour office windows at upper levels.
- Internal chimney breasts to be removed, external stacks should be retained

- Existing problem of noise and anti-social behaviour from Ashley Rd at night including music from venues and bottle bins being emptied – consider existing residents
- The sound readings were **NOT** taken at the nearest property but actually down the road. We (168 Ashley Rd) are the nearest property; we're adjacent to 166 Ashley Road on 3 floors.
- The timing of the readings at a quarter to 11pm at night until a quarter to midnight on a Wednesday. In our opinion (as we work here), this was Hale's quiet day of the week and at that time certainly very quiet. If it had been taken on a Thursday to Sunday night very different readings.

### Parking

- Parking to the rear restricted; patrons to the restaurant could block private parking spaces
- Service vehicles will cause disruption when on Cecil Road (Can't stop on Ashley Road)
- The proposal will result in an increase in trips by car to the venue

### General Issues

- This part of Hale is characterised by estate agents, law firms, financial planners, opticians and boutique shops, if approved then the only part of Hale with no restaurants will be removed
- Local businesses create well paid long term jobs for local people.
- Clients of Now Planning object, many are Hale residents who expect privacy when dealing with their financial matters
- The village is losing retail shops at an alarming rate to restaurants and coffee shops; soon there will be no other reason to visit Hale other than in the evening.
- Air conditioning units and flue will result in unsightly features on the building on this prominent site.

## **OBSERVATIONS**

### PRINCIPLE OF DEVELOPMENT

1. The proposed development involves a change of use of the premises from the existing A1 (retail) at ground floor and B1/A2 (Office) at first floor. Of relevance with regards this proposal is policy W2.11 within the Core Strategy, and in particular the following sections:-
  - Sustainable urban design will be a priority, with a particular emphasis on encouraging a mix of uses appropriate to the centre, active frontages and high quality in the design and finish of the public realm; and
  - Changes of use from A1 retail to other uses should be carefully considered in terms of their impact on the function, character, vitality and viability of the centre

as a whole and on specific frontages particularly within primary shopping frontages.

2. Part of the proposed works includes a new frontage to the premises at ground floor facing onto Ashley Rd. As stated earlier the site is identified being within a main shopping frontage along this section of Ashley Road. The proposed new frontage will involve a hardwood timber frame glazing system, with similar proportions to the existing shop frontage. The main pedestrian access will be flipped to the southern extremity of the front elevation, however this is not considered to unbalance the shop frontages with a number of premises having main entrance openings on differing parts of the front elevations. The design and materials suggested are considered appropriate in this location (subject to seeing samples prior to installation) and would therefore be considered as a positive addition to the street frontage.
3. The proposed development involves a mixed use of retail (A1) and restaurant (A3) although it is acknowledged the main use of the premises will relate to the restaurant use. The loss of this A1 unit is not considered to undermine the core retail function of the Hale District Centre and the proposed use would be appropriate in terms of the function and character of the centre. It is therefore considered to be consistent with Core Strategy Policy W2.11. Whilst not applicable to the development proposed, it should be noted that the General Permitted Development Order 2015 at Part 3, Class C does allow the change of use from shops to restaurants subject to conditions, including an application for prior approval.
4. Whilst the site is not within the Hale Station Conservation Area, it has been identified as an area that is proposed to be included within the current on-going Conservation Area Appraisals and specifically to be included within Hale Station Conservation Area boundaries.

## IMPACT ON STREETSCENE

5. The front elevation of the building facing Ashley Road has a first floor bay window and timber sash windows, slate roof with a buff and brown/red brick detail; this reflects the first floors of the majority of premises along this side of Ashley Rd. There are no proposals to alter the building fabric at first floor level as part of the redevelopment of the premises.
6. The existing ground floor shop frontage comprises a timber frame main shop window (floor to ceiling glazing) with a low level stall riser and plinths at each end of the shop frontage, the premises has a non-illuminated fascia sign, all of which finished in a neutral grey colour. The proposal to the front elevation will involve introduction of a hardwood timber window frame with similar window proportions as existing. The applicant has indicated a desire to paint finish the new timber frame and door, however no colour has been suggested, therefore submission of

such details would be dealt with through an appropriate submission of materials condition.

7. The premises have an existing hanging externally illuminated projecting sign and non-illuminated fascia sign. The plans submitted indicate the intention to use the same existing fascia sign and hanging sign with new company logos, however a separate consent to display advertisements would be required.
8. To the rear of the building, which faces directly onto Cecil Road, a number of the premises which front Ashley Road have their rear elevation facing Cecil Rd, however there are a number of business which have their main entrance from Cecil Rd (i.e. their front elevation). The Cecil Rd elevation of the application site comprises a two storey outrigger with a mono-pitch roof, attached to the rear of the two storey outrigger is a small lean to single storey outrigger. An area of hardstanding is located to the rear of the site and opens out directly onto the pavement of Cecil Road (no physical boundary with Cecil Road).
9. The proposed works will involve a single storey extension projecting from the main two storey rear elevation and which will measure approximately 1.6m in width and extending along the flank elevation of the two storey outrigger (a distance of approximately 4.9m). The extension will measure approximately 2.2m to eaves and 3m to ridge line; it has been set in from the boundary with 1a Cecil Rd by approximately 0.1m to prevent any overhang. 1A Cecil Road is an office use, TFC Ltd and Beaumont Lord Ltd (Commercial Property Agents/Surveyors). The main entrance to 1a Cecil Rd is via a pedestrian door entrance at ground floor level on the two storey outrigger serving that premises. A small side window on the outrigger faces towards where the new single storey extension would be situated; this window is believed to serve a stairwell/entrance area. Although visible from Cecil Rd, the extension would not project out beyond any established building line and is similar in size to the existing single storey outrigger to the rear of the premises. Appropriate matching materials would be controlled by way of appropriate condition to ensure suitable match. The extension would not have any adverse impact on the streetscene. The bin store compound is proposed to be timber hit and miss type fencing approximately 1.5m in height; this screen would be considered acceptable in this location and would not detract from the character of the streetscene. An existing boundary wall between 166 and 168 Ashley Rd (to the rear) will form part of the bin compound.
10. In addition to the extension it is proposed to install three air conditioning units on the rear elevation of the two storey outrigger (below first floor window cill level) one of the a/c units is currently situated on the side of the two storey outrigger of the premises and would be repositioned to the rear elevation. In addition a kitchen extract flue and an intake cowl are proposed on the roof slope of the two storey outrigger. The Cecil Road elevation of the buildings which also front onto Ashley Road, include a variety of outriggers with assorted signage, building design and external attachments such as air-conditioning units and metal roller

shutters. Whilst the air conditioning units will be visible from Cecil Road, in the context of the surrounding commercial premises they would not be considered so harmful to the streetscene to justify a refusal of planning permission.

## RESIDENTIAL AMENITY

11. The nearest residential property to the application site is 14, 14a and 16 Cecil Rd to the rear of the site on the opposite side of Cecil Rd with an approximate separation distance of approximately 20m from the front elevation of 14-16 Cecil Rd to the rear of the application premises; 14 and 16 Cecil Road is a conventional pair of semi-detached dwellings, with No.14 converted into two flats. The residential properties face towards the rear elevations of the commercial premises fronting Ashley Rd. Immediately to the rear of the site are two detached office blocks Cecil House and Campaign House both of which appear to be occupied by law firms. Immediately to the north-east side of the site on the opposite side of Cecil Rd are the Dee Thai restaurant and the Earle restaurant. Within the commercial terrace either side of the application property is Now Financial Planning at 168 Ashley Rd who occupy that entire building; 164 Ashley Rd is occupied by John Hilditch estate agents who occupy the ground floor with TFC Ltd and Beaumont Lord at the upper level.
12. The applicant has undertaken a noise assessment which has been considered by the Councils Pollution, Housing and Licensing section. The proposal has been considered as an appropriate form of development in this location with regards noise subject to the inclusion of two conditions; one of which would involve the provision of the noise mitigation measures, namely the inclusion of attenuators and acoustic enclosures to the air conditioning units and restricting the hours of refuse collections between 7.30am – 21.00pm Monday – Friday; 09.00am – 21.00pm Saturdays and no collections on Sundays or Bank Holidays. Whilst the proposal site is located close to residential properties on Cecil Road, there are a number of nearby restaurants in this location. This part of Hale district centre has a number of residential properties on the edge of the commercial area of the centre and therefore the activity associated with such food premises is established in this area.
13. The neighbouring office uses at 168 and 1a Cecil Rd have raised concerns regarding the impact of noise, odour and general activity of the proposed development upon their businesses. Whilst the concerns of the businesses are acknowledged, the adjacent premises are not residential but have a commercial use. Nonetheless the applicant will be required to undertake the necessary noise mitigation measures on the external plant. In addition the applicant in their submission has stated that the bin store will hold 4no. 240 litre bins; the bin company undertakes its own recycling so there is no need for separate bins; the bin operator will have a key to the bin compound with no plastic bags/waste left kerb side; collection times would be 10.00am and 17.30pm. The applicant has also stated that no transfer of waste will occur after 20.00pm (i.e. emptying of



glass bottles). The proposed kitchen extract is proposed on the roof of the two storey rear outrigger. The ridgeline of this outrigger is higher than the ridgeline of the adjoining outrigger at 168 Ashley Rd; it is also at a higher level than the first floor windows at 1a Cecil Rd. A second floor window is located on a rear facing gable above 162-164 Ashley Rd, however this window faces directly towards Cecil Rd. Pollution and licensing have raised no objections to the proposal with regards odour, an appropriate condition to be attached requesting details fume extraction system to be installed to ensure no odour nuisance to residential properties.

14. The proposed new extension indicates a new pedestrian door access to the rear courtyard area onto Cecil Road. Whilst this area to the rear has been identified as the location for the bin store and servicing, it is considered appropriate to prevent public access to the premises from the Cecil Rd entrance and also prevent any sitting out area to be formed in this area. This can be controlled by condition. It is also considered appropriate to request that a management plan be submitted via an appropriate condition which outlines how the applicant will ensure behaviour and noise to be controlled; stipulate waste management procedure and also preventing staff/customers congregating at the rear to smoke etc. (this is not an exhaustive list)

#### PARKING AND SERVICING

15. The application premises which are currently vacant did have two cars parked to the rear at the time of the officer's visit to site, one of which partially overhung the pavement. The applicant has indicated that one car-parking space exists to the rear and would still be retained following the development. Following the erection of the extension and bin store at the rear, this area could accommodate a car or small delivery van parked side-on. The applicant proposes that deliveries are undertaken from Cecil Rd. The side of Cecil Rd that the application site backs onto has single yellow line road marking, with double yellows on the opposite side. The LHA have confirmed that the single yellow line has a 'No waiting' restriction Monday – Saturday 8am – 6pm). The application site is located close to Cecil Rd public car park within a district centre and there for is considered to be in a sustainable location with regards parking provision. The site is also located close to Hale railway station. The LHA have requested that the applicant provide two secure cycle spaces for staff.

#### DEVELOPER CONTRIBUTIONS

16. This development is considered not to require any planning obligations.

#### **RECOMMENDATION: GRANT subject to the following conditions:-**

1. Standard
2. Approved Plans

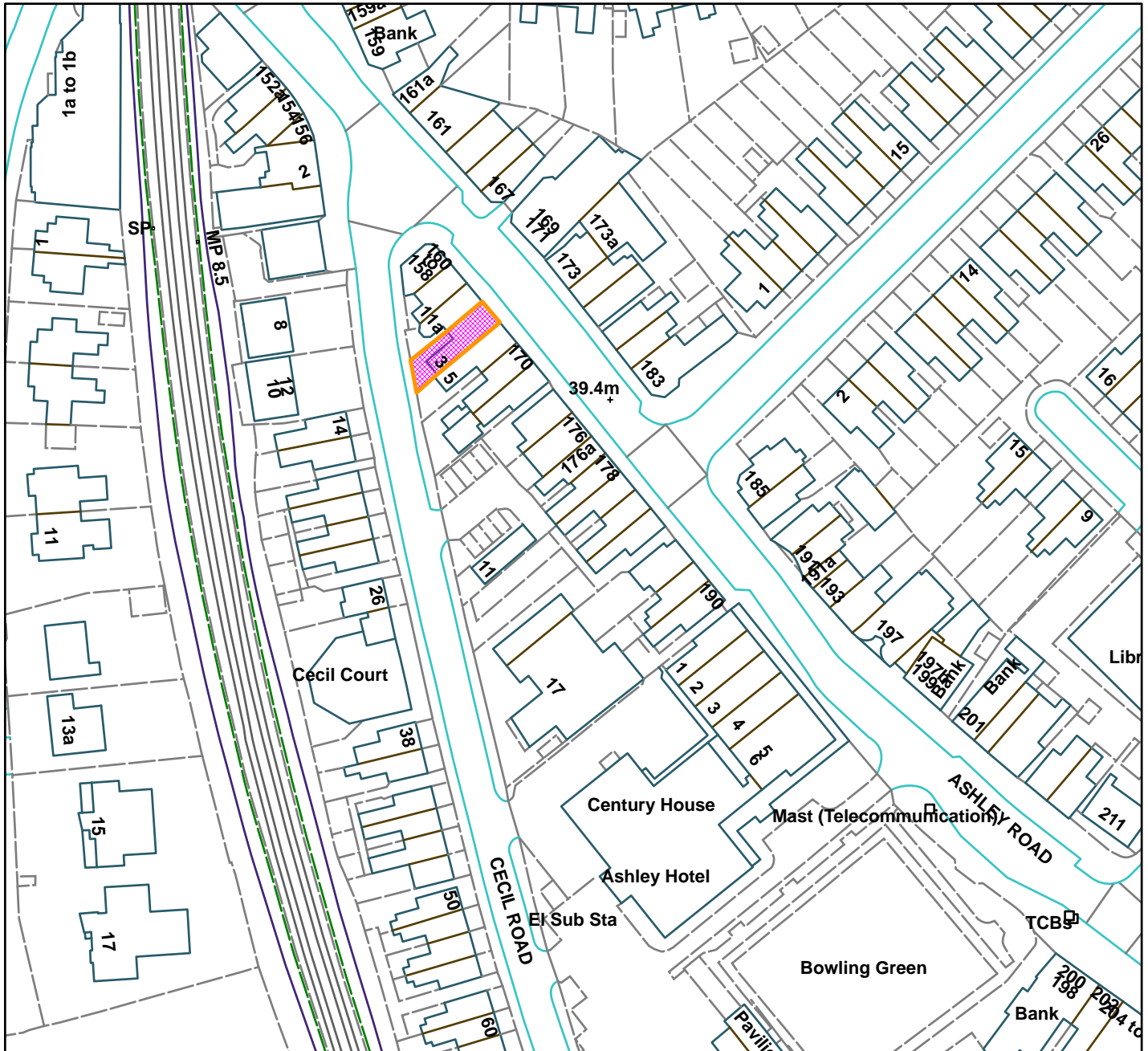
3. Submission of materials
4. Opening Hours – 1000 to 2330 Monday to Saturday and 1000 to 2200 Sundays and Bank Holidays
5. Refuse collection not to be outside the hours of 0730 to 2100 Monday to Friday, 0900 to 2100 Saturday and at no time on Sundays or Bank Holidays
6. Noise mitigation measures
7. Submission of extract Flue system
8. No access/exit to/from the premises by members of the public from Cecil Rd
9. No external dining area to be formed to rear of premises
10. Submission of management plan.
11. Submission of cycle parking provision.

CM

---



166 Ashley Road, Hale (site hatched on plan)



**Scale:** 1:1,250

Reproduced from the Ordnance Survey map with permission of the Controller of Her Majesty's Stationery Office © Crown Copyright 2012.

Unauthorised reproduction infringes Crown Copyright and may lead to prosecution or civil proceedings.

Organisation	Trafford Council
Department	Planning Service
Comments	Committee date 13/08/2015
Date	03/08/2015
MSA Number	100023172 (2012)

**WARD:** Flixton

**85087/FUL/15**

**DEPARTURE: NO**

Demolition of vacant Dairy buildings and erection of a part two-storey, part three storey development of Use Class C2 Residential Accommodation with Care comprising of 56 apartments for the elderly with associated access roads, car parking, landscaping, boundary treatments and services.

**Former Creamline Dairies, 181 Moorside Road, Flixton, M41 5SJ**

**APPLICANT:** Gladman Care Homes Ltd and Creamline Dairies Ltd

**AGENT:** Mrs Penny Smith - Gladman Care Homes Ltd

**RECOMMENDATION: GRANT**

---

### **SITE**

The 0.7 hectare site is situated to the south of Moorside Road in an established predominantly residential area of Urmston. The site is currently vacant and comprises the buildings and yards associated with the former Creamline Dairies business. The site is predominantly flat with a slight gradient towards the southern part of the site. There are two large mature trees located to the northern boundary of the site.

To the north of the site is Moorside Road and a terraced row of shops and local services. To the north of Moorside Road is Trafford General Hospital. There are residential properties located to the west on Jackson Crescent, the south on Bowfell Road and to the east of the site on Lostock Avenue. Vehicular access to the site is taken from Moorside Road.

### **PROPOSAL**

Planning permission is sought for the demolition of the former dairy buildings and the erection of a residential apartment scheme with associated care facilities. The scheme proposes 56 apartments comprising the following mix of units:-

- 10 x one bed studio apartments (35 sq.m)
- 14 x one bed apartments (56 to 59 sq.m)
- 28 x two bed apartments (74 to 91 sq.m)
- 4 x three bed apartments (101 to 113 sq.m)

The development will have communal facilities including:-

- Residents lounge
- Restaurant
- Care managers office, care team office and staff facilities for 24 hour care
- Reception and small shop for residents
- Hairdressers

- Multi-purpose room
- Spa room with assisted bath and therapy area
- Guest suite
- Internal mobility scooter store
- Amenity space and landscaping

The supporting information which accompanies the application indicates that the facility will be targeted towards the over 60's.

The existing vehicular access arrangements off Moorside Road will be remodelled and used to access the site. 28 parking spaces (including 4 disabled spaces) are provided on site for residents, care staff and visitors.

The gross internal floor area of the scheme is 5,660 sqm

### **THE DEVELOPMENT PLAN IN TRAFFORD**

The Development Plan in Trafford Comprises:

- The **Trafford Core Strategy**, adopted 25th January 2012; The Trafford Core Strategy is the first of Trafford's Local Development Framework (LDF) development plan documents to be adopted by the Council; it partially supersedes the Revised Trafford Unitary Development Plan (UDP), see Appendix 5 of the Core Strategy.
- The **Revised Trafford Unitary Development Plan (UDP)**, adopted 19th June 2006; The majority of the policies contained in the Revised Trafford UDP were saved in either September 2007 or December 2008, in accordance with the Planning and Compulsory Purchase Act 2004 until such time that they are superseded by policies within the (LDF). Appendix 5 of the Trafford Core Strategy provides details as to how the Revised UDP is being replaced by Trafford LDF;
- The **Greater Manchester Joint Waste Plan**, adopted 01 April 2012. On 25<sup>th</sup> January 2012 the Council resolved to adopt and bring into force the GM Joint Waste Plan on 1 April 2012. The GM Joint Waste Plan therefore now forms part of the Development Plan in Trafford and will be used alongside district-specific planning documents for the purpose of determining planning applications; and
- The **Greater Manchester Joint Minerals Plan**, adopted 26th April 2012. On the 13th March 2013, the Council resolved that the Minerals Plan, together with consequential changes to the Trafford Policies Map, be adopted and it came into force on the 26th April 2013. The GM Joint Minerals Plan therefore now forms part of the Development Plan in Trafford and will be used alongside district specific planning documents for the purpose of determining planning applications.

## **PRINCIPAL RELEVANT CORE STRATEGY POLICIES**

L1 – Land for New Homes  
L2 – Meeting Housing Needs  
L4 – Sustainable Transport and Accessibility  
L5 – Climate Change  
L7 – Design  
L8 – Planning Obligations  
R2 – Natural Environment

## **PROPOSALS MAP NOTATION**

None

## **PRINCIPAL RELEVANT REVISED UDP POLICIES/PROPOSALS**

None

## **NATIONAL PLANNING POLICY FRAMEWORK (NPPF)**

The DCLG published the National Planning Policy Framework (NPPF) on 27 March 2012. The NPPF will be referred to as appropriate within the report.

## **RELEVANT PLANNING HISTORY**

**H03846** - Erection of storage building for milk distribution depot. Application approved 20.07.1976

## **APPLICANT'S SUBMISSION**

The applicant has submitted the following documents in support of the planning application:-

- Arboricultural Assessment
- Carbon Budget Statement
- Care Statement
- Construction Dust Assessment
- Design and Access Statement
- Desk Study Assessment Report
- Ecological Appraisal
- Flood Risk Assessment
- Planning Statement
- Parking Statement
- Transport Assessment
- Travel Plan Statement

## **CONSULTATIONS**

**Greater Manchester Ecology Unit** – No objections to the scheme on nature conservation grounds. They advise that bats can and do turn up in unlikely places. If bats are found at any time during works should cease immediately and advice sought from a suitability qualified person about how best to proceed.

**Lead Local Flood Authority** – No objections subject to a condition to limit peak discharge rates of storm water in accordance with the Council's Level 2 Hybrid Strategic Flood Risk Assessment.

**Local Highway Authority** – No objections. The proposed development does not generate a significant number of vehicle trips, whilst the number of HGV movements will be significantly less than that associated with the previous use as a dairy. A minor amendment to the central reserve on Moorside Road will be provided to facilitate the right-turning movements into and out of the site. In addition the proposals include a footway link into the site (along the eastern side of the site access) connecting to the footway on Moorside Road which are supported by the LHA.

The LHA state that the Council's parking standards contain different levels of parking provision for the two types of development falling within use Class C2; for residential care homes / nursing homes of this size a maximum car parking provision of 30 spaces is required; whereas sheltered accommodation requires 18 spaces. The applicant considers the proposals are a combination of these uses and proposes 24 car parking spaces (plus 4 disabled parking bays). It may be difficult to classify the proposals firmly within either of these categories of development, but it is considered that even a deficiency of 6 car parking spaces compared to the Council's maximum standards would not provide a robust basis to justify a recommendation for refusal. Some on-street parking is available in the immediate vicinity of the site, but this is in high demand throughout the day by staff and visitors to Trafford General Hospital. The parking close by on Moorside Road in front of the shops is limited to short stay parking only. Details of the location of the cycle parking and the type of stands to be used are required prior to any development commencing. Details of servicing and refuse vehicles have been submitted as part of the application which demonstrate that appropriate access arrangements are available as part of the scheme.

**Pollution – Contaminated land** – No objections subject to a condition to secure the submission of a Phase One Contaminated Land Report

**Pollution and Licensing** – No objections subject to the submission and implementation of a dust management plan, details of the proposed extraction system; fixed plant details and external lighting details.

**United Utilities** – No objections. They recommend that the site is drained on a separate system and that no surface water is discharged either directly or indirectly to the combined sewer network. These matters can be secured via conditions.

## **REPRESENTATIONS**

11 objections have been received as a consequence of the planning application publicity.

- Loss of amenity in respect of overlooking, privacy and loss of light
- Increased traffic and congestion
- Impact on highway safety
- Increased parking pressures on local roads
- There should be no vehicular or pedestrian access taken from Lostock Avenue
- Over development
- Development is out of scale and character with the local area
- Inappropriate materials
- Detrimental impact on the visual amenity of the area
- Existing boundary walls should be retained as they provide privacy and security to resident's properties and gardens
- Proposed boundary landscaping (trees) would reduce light
- More trees should be planted to site boundaries
- Additional debris and root / branch issues from trees
- Details of asbestos roof clearance requested
- External lighting
- Air pollution
- Age of occupiers should be controlled via planning conditions
- The use of the building should be controlled to restrict development allowed under permitted development process
- Communal facilities duplicate facilities found within Urmston Town Centre
- Impact on house prices

A petition, signed by 36 residents has also been received which has raised the following objections:-

- The area would be overdeveloped and not in proportion with the allocated plot and surrounding properties
- The proposed two and three storey build would be overlooking and over dominant, resulting in loss of privacy due to the large number of windows of the proposed development
- The proposed building is a substantial build which is completely out of scale and character with the local area, as existing buildings are of 1930 style and are two storey, three bedroom residential properties, not apartments. The proposed build would thus be detrimental to the visual impact and amenity of the area
- The proposed build would significantly impact on traffic congestion around the area, as only 28 car parking spaces (including 4 disabled spaces) will be provided
- It is unclear if the apartments are being solely built for the elderly through the life cycle of the proposed build, which thus requires this to be written into the planning terms and conditions should there be a change in ownership / purpose of the buildings in the future



- The communal facilities that are proposed within the development include residents lounge, restaurant, kitchen, spa room, guest suite, hair salon, multi-purpose room and landscaped gardens. However, the building occupies the entire area with minimal landscaped gardens. Also the communal facilities are not on keeping with the local area and will be duplicated as similar facilities already exist in the recently regenerated Urmston Town Centre.

## **OBSERVATIONS**

### **PRINCIPLE OF DEVELOPMENT**

#### **i) Strategic location**

1. The site is unallocated on the UDP Proposals Map and is vacant following the closure and relocation of the dairy business to Trafford Park. The former buildings however remain in situ and will be demolished, should planning permission be granted. The site meets the definition of previously developed land outlined in Annex 2 of the NPPF and as such will secure sustainable development through promoting the reuse of resources in accordance with Core Strategy Strategic Objective SO7.
2. NPPF paragraph 17 identifies a set of twelve core land use planning principles, of which bullet point 8) states that planning should “*encourage the effective use of land by reusing land that has been previously developed (brownfield land), provided that it is not of high environmental value.*” The ecological value of the site is addressed in detail in subsequent sections of this report; however the site is not of high environmental value and as such the reuse of this previously developed site is welcomed and is in accordance with the key planning principles outlined in NPPF paragraph 17.

#### **ii) Proposed uses**

##### **a) Extra Care Facility (C2)**

3. Core Strategy Policy L1 seeks to release sufficient land to accommodate a minimum of 12,210 new dwellings (net of clearance) over the plan period to 2026. The policy states that this will be achieved through the delivery of new build, conversion and sub division of existing properties. Core Strategy policy L2 indicates that all new residential development proposals will be assessed for the contribution that will be made to meeting the housing needs of the borough, whilst Core Strategy Policy L2.18 makes specific reference to the ‘frail elderly’ of the Borough, the Council will seek to meet their needs through allowing 4% (approximately 500 units) of the overall housing land target to be developed as new housing for older person households, suitable for a range of household circumstances (tenure and type), including ‘extra care’ housing.
4. The applicant has submitted a significant amount of information in support of their application to demonstrate that the proposed use comprises a C2 use (residential institutions) rather than a C3 use (dwelling houses) as defined by the Town and Country Planning Use Classes Order. The proposed facility

provides a form of accommodation that offers care and support to aged and frail people within their own home. A development of this type would typically incorporate a range of facilities including restaurant, kitchen, lounge, and laundry and staff accommodation, all under one roof. The development will provide accommodation for individuals or couples within a self-contained home, but where facilities are shared, such as lounges and restaurants and where care services and support are easily accessible. It is concluded that the proposed use comprises a C2 use as the primary purpose of the building is to provide residential accommodation and care to people in need of care. It is considered that the care element of the scheme is a reason why people choose to live there, and as such is an integral part of future occupiers everyday lives. Planning conditions are proposed to ensure that the proposed building is used for the designed purpose of providing self-contained independent units of accommodation for people who are contracted into a care package and who have a minimum age of not less than 60 years of age.

#### b) Mix, type, size

5. Paragraph 50 of the Framework states that local authorities should plan for a mix of housing based on current and future demographic trends, market trends and the needs of different groups in the community. Core Strategy policy L2.6 indicates that the proposed mix of dwelling types and sizes should contribute to meeting the housing needs of the Borough as set out in the Council's Housing Strategy and Housing Market Assessment. Policy L2.7 states that 1 bed accommodation will, normally, only be acceptable where schemes support the regeneration of Trafford's Town Centres and the Regional Centre.
6. The proposed scheme will deliver 56 apartments comprising the following mix and size of units:-
  - 10 x one bed studio apartments (35 sq.m)
  - 14 x one bed apartments (56 to 59 sq.m)
  - 28 x two bed apartments (74 to 91 sq.m)
  - 4 x three bed apartments (101 to 113 sq.m)
7. The site is neither located within the Regional Centre or within one of Trafford's four town centres. However, the proposed mix of units is considered acceptable as the provision of new apartments would contribute to a balanced mix of dwellings in the local area given that the dwelling stock in the surrounding area generally comprises of houses. The mix and size of units proposed has been determined by current market demand which the applicant has ascertained through market testing.

#### c) Affordable housing

8. The proposed development constitutes a C2 use and as such there is no policy requirement within the Development Plan for the provision of affordable housing as part of the proposed scheme.

#### d) Ancillary facilities

9. The proposed scheme makes provision for a café and a room to be used by a mobile hairdresser, chiropodist, and other therapists to deliver well-being services to the residents. The proposed café will be used by residents and their families / visitors, staff and carers for dining. None of these facilities will have 'public' access due to the Extra Care and vulnerable nature of the residents of the scheme and as such the proposed uses are therefore ancillary to the main use of the building. In light of the ancillary nature of these uses, it is not considered necessary to impose any planning conditions which restrict the use of these facilities to occupants of the building.

### **DESIGN, LAYOUT, SCALE AND MASSING**

10. The NPPF attaches "*great importance to the design of the built environment*" and indicates that, "*good design is a key aspect of sustainable development.*" NPPF states that, "*permission should be refused for development of poor design that fails to take the opportunities available for improving the character and quality of an area and the way it functions.*" Core Strategy policy L7 reiterates these policy guidelines and states that high quality design is, "*a key element in making places better and delivering environmentally sustainable developments.*" The policy provides policy guidance in respect of design quality, functionality, amenity, security and accessibility.
11. The site will be accessed via a single point of access off Moorside Road, whilst vehicle parking is provided by two parking courts, one which is located to the front of the site and second adjacent to the entrance court. The building will be located centrally within the site and as such does not have a strong presence in the streetscene. Notwithstanding this, mature trees to the frontage of the site are retained as part of the scheme and it is considered that the footprint of the building responds appropriately to the constraints of the site. The main entrance and parking court is located centrally within the site to create an entrance court. The main ground floor communal uses, including the reception area and restaurant overlook the entrance court and will create opportunities for natural surveillance and active frontages. The building is set within a landscaped garden, which incorporates both public and private spaces. It is considered that the proposed layout is logical and appropriate to its context and will provide a high quality scheme set within landscaped grounds.
12. The applicant has submitted a series of elevational drawings which demonstrate how the proposed building relates to its context. The proposed scheme includes two and three storey elements and has been designed so that the two storey elements are of a comparable scale to the adjacent existing two storey dwellings, whilst the 3 storey elements of the building are focused within the central and northern parts of the building and are of a comparable scale to commercial / residential properties on Moorside Road. The proposed ridge height and eaves height are comparable to the properties on Moorside Road and it is therefore considered that the scale of the building

is appropriate given its surrounding context and its relationship to adjacent buildings and site topography.

13. The applicant has successfully broken up the mass of the proposed elevations by using a varied palette of materials. Details submitted as part of the application indicate that two red facing bricks will be used to the ground and first floor, whilst grey render will be used to the second floor. The scheme incorporates a smooth red facing brick string course between floors to emphasise the change in materials and to add architectural interest. The submitted elevations are considered appropriate and are in keeping with the surrounding area and wider context of the site. A planning condition is proposed to ensure that high quality facing materials are secured.

## **RESIDENTIAL AMENITY**

14. The 'New Residential Development' Supplementary Planning Guidance (SPG) identifies a series of spacing guidelines for new residential development. Paragraph 11.2 of the SPG indicates that for two storey dwellings the minimum distance between dwellings which have major facing windows is 21 metres across public highways and 27 metres across private gardens; or where three storey dwellings are proposed, the minimum distances are increased by 3 metres. The SPG states that private rear garden areas should not be closely overlooked and that distances to rear garden boundaries from main windows should be at least 13.5 metres for 2 storey flats or houses or flats with 3 or more storeys. Paragraph 11.1 of the SPG however qualifies this advice stating that rigid application of the spacing standards can stifle creativity in design and result in uniformity of development and therefore a flexible approach to privacy distances should be adopted.
15. Objections have been received from 28, 30, 32 and 38 Bowfell Road; 4, 6 and 7 Lostock Avenue; 39 Malvern Avenue; and 171 Moorside Road in respect of the impact that the proposed development will have on the privacy of existing residents. The applicant has submitted a site layout plan identifying a number of key interface distances between the proposed building and the existing dwellings. The building has relationships with a number of properties on Lostock Avenue, Bowfell Road, Moorside Road, and Jackson Court all of which have been assessed, however the key impacts are assessed in more detail below:- .

Lostock Avenue – The eastern elevation of the proposed building comprises three balconies and one Juliet balcony located at second floor level. In respect of 8 Lostock Avenue, the proposed balcony has been amended to a Juliet balcony and the distance to the site boundary has therefore been increased to 14 metres and as such meets the requirements of the guidance. In respect of 11 Lostock Avenue, balconies are located between 14 and 17 metres from habitable room windows. Due to the proximity of the balconies to the habitable room windows, the applicant has amended the scheme to incorporate visibility screens to these balconies. It is considered, on this basis that an acceptable level of amenity will be retained.

Bowfell Road – The elevation to Bowfell Road, comprises both 2 and 3 storey elements. All balconies and habitable room windows are located between 27 and 48 metres from existing habitable room windows, whilst the distance to the boundary of the site ranges from 13.8 to 18.7 metres and on this basis it is considered that the proposed development will not have an unacceptable upon the amenity for existing residents. With particular reference to 38 Bowfell Road the distance between facing windows to the proposed and existing dwellings is 51 metres, whilst the distance to the site boundary is 13.8 metres. The applicant has confirmed that windows serving bathrooms will be obscurely glazed and this is secured via condition.

Jackson Court – The elevation to Jackson Court, comprises predominantly 2 storey elements and some 3 storey elements. There are three balconies located to this elevation, two of which face directly towards Jackson Court. The balconies and habitable room windows are located in excess of 13.5 metres from the site boundary and there are no face to face habitable room relationships created. On this basis, it is considered that the proposed development will not have an unacceptable upon the amenity for existing residents.

Moorside Road – This elevation comprises predominantly 3 storey elements, however those parts of the elevation located closest to the properties on Moorside Road comprise either blank elevations or secondary windows. The proposed balconies are located approximately 31 metres from directly facing habitable room windows on Moorside Road. On this basis, it is considered that the proposed development will not have an unacceptable upon the amenity for existing residents.

16. The layout of the scheme will provide future occupiers with adequate light and outlook from habitable room windows. The vast majority of apartments provide outside amenity space in the form of a balcony, in addition to which there is a communal garden provided within the scheme layout. It is therefore considered that the proposed development complies with Core Strategy policy L7.

## **HIGHWAYS AND PARKING**

17. NPPF paragraph 32 states that Transport Assessments should be prepared for all developments that generate significant amounts of traffic. The Transport Assessment should identify opportunities for sustainable transport modes; ensure that safe and suitable access to the site can be achieved for all people; and that improvements to the highway network are provided that would limit any significant impacts of the development. NPPF states that, *“development should only be prevented or refused on transport grounds where the residual cumulative impacts of development are severe.”* The applicant has submitted a Transport Statement (TS) in accordance with the requirements of the Framework.

### i) Proposed access arrangements

18. The existing vehicular access arrangements off Moorside Road will be remodelled and used to access the site. The proposed site access arrangements are outlined on drawing 1411/01/A and comprise of a 4.8 metre wide site access carriageway with 6.0 metre corner radii. The proposed access arrangements will introduce a designated right turn facility within an existing break in the central reservation. The turn right facility will introduce white line markings clearly showing the marked separation of the right turn lane for the hospital and the proposed development. In addition the proposals provided for a footway link into the site, along the eastern side of the site access. The Local Highway Authority is supportive of the proposed access arrangements and highway improvement works. These matters will be secured via a S278 Agreement.

### ii) Impact on the highway network

19. The TS provides an assessment of the impact of the development on the highway network. The TS states that the proposed development will generate 8 two way movements in the AM peak and 9 two way movements in the PM peak. The proposed development is estimated to generate a net decrease of 3 two way movements and a net increase of 9 two way movements in the AM and PM peak hours respectively. The TS therefore concludes that overall, the proposed development has no material traffic impact on the local highway network. The Local Highway Authority has reviewed the TS and they confirm that the proposed development does not generate significant numbers of vehicle trips and that in view of the previous use; the number of HGV movements will be substantially less. The Local Highway Authority raises no objections to the scheme in terms of impact upon the highway network.

### iii) Parking

20. Policy L4.14 and Appendix 3 of the Core Strategy, supplemented by the Parking Standards and Design Supplementary Planning Document (SPD) identify the parking standards for a range of development types across the borough. The SPD identifies three accessibility areas which cover various parts of the Borough. Urmston is identified as being located within Area C.

21. The applicant has submitted a Parking Statement in support of the application. The document provides an analysis of parking by residents, staff and visitors at other similar developments; concludes that none of the standard categories in SPD3 fully apply to the type of development proposed; state that the quantum of parking is within the established ranges of precedent schemes; and concludes that the level of parking proposed is sufficient for the proposed use and will be appropriate for residents, staff and visitors using the development.

22. The Council's parking standards contain different levels of provision of parking for the two types of development falling within use Class C2; for residential care homes / nursing homes of this size a maximum car parking provision of

30 spaces is required; whereas sheltered accommodation requires 18 spaces. The applicant considers that the proposals are a combination of these two uses and proposes 24 car parking spaces (plus 4 disabled parking bays). The Local Highway Authority note that it is difficult to classify the proposals firmly within either of these categories of development, however they consider that even a deficiency of 6 car parking spaces compared to the Council's maximum standards would not provide a robust basis to justify a recommendation for refusal. They indicate that some on-street parking is available in the immediate vicinity of the site, but this is in high demand throughout the day by staff and visitors to Trafford General Hospital. They also note that parking on Moorside Road in front of the shops is limited to short stay parking only.

23. The scheme makes provision for two motorcycle spaces in accordance with policy requirements. The scheme makes provision for 4 cycle spaces which exceeds the policy requirement.

#### iv) Travel Plan

24. NPPF paragraph 36 states that all developments which generate significant amounts of transport movements should be required to provide a Travel Plan. Core Strategy policy L4.13 states further that the Travel Plan should include measures to reduce congestion, improve road safety and promote public transport, walking and cycling as part of an integrated approach to managing travel demand. It is important to influence travel patterns at the beginning of occupation and therefore the development should be accompanied by a robust Travel Plan. The Travel Plan should feature a range of measures promoting a choice of transport mode, and a clear monitoring regime with agreed targets. The applicant has submitted an interim Travel Plan in support of the planning application which sets out the principles and range of measures which will form part of the final Travel Plan and provides details of its management, implementation and monitoring. It is considered that the submitted document provides a useful starting point for the preparation and submission of a full Travel Plan document which can be conditioned as part of this planning permission, if granted.

### **ECOLOGY, LANDSCAPING AND TREES**

25. The applicant has submitted an Ecological Appraisal (EA) which provides details of the Extended Phase I survey and initial protected species surveys including internal / external bat surveys. The EA identified no evidence of roosting bats within any of the buildings, whilst these buildings are considered to provide negligible / no potential to support bats given the lack of roosting opportunities. Greater Manchester Ecology Unit (GMEU) have reviewed the EA and raise no objections to the development on nature conservation grounds. They do however note that bats can turn up in unlikely places, and therefore if any bats are found then works should cease immediately. It is recommended that this advice is provided to the applicant in the form of an informative.

26. The applicant has submitted a Landscape Strategy in support of the planning application. The Strategy includes indicative details of hard and soft landscaping, boundary treatments, surface treatments and furniture. The Design and Access Statement identifies a series of landscape objectives which are carried forward in the Landscape Strategy. Concerns have been raised by some residents with regards to tree planting to the boundary of the site and the retention of existing boundary treatments adjacent to properties on Bowfell Road. At this stage, details of tree planting are indicative only with no specific species identified and as such landscaping details will be secured via condition. The applicant has indicated that the proposed landscaping scheme and boundary details will be discussed with neighbouring residents prior to its submission to the Local Planning Authority for approval. In light of this, and whilst the principle of the landscape strategy is accepted, it is recommended that conditions are attached to secure the submission of a detailed landscape scheme and its long term maintenance and the provision and retention of boundary treatments.
27. The applicant has submitted an Arboricultural Assessment in support of the planning application. The Arboricultural Assessment presents an assessment of the existing trees arboricultural values and provides an assessment of the impact arising from the proposed redevelopment of the site.
28. The Tree Survey classifies the trees on site into the following categories: -
- Category A:** Trees of high quality and value: in such a condition as to be able to make a substantial contribution (a minimum of 40 years is suggested). Trees in this category are the best trees on the site and should be retained where possible.
- Category B:** Trees of moderate quality and value: In such a condition as to make a significant contribution (a minimum of 20 years is suggested).
- Category C:** Trees of low quality and value: Currently in adequate condition to remain until new planting could be established (minimum of 10 years is suggested), or young trees with a stem diameter below 150mm.
- Category U:** Trees in such a condition that any existing value would be lost within 10 years and which should, in the current context, be removed for reasons of sound arboricultural management.
29. The Arboricultural Assessment identifies 15 individual trees (T1 – T15) and 2 groups (TG1 and TG2). No trees on site are covered by a Tree Preservation Order. To facilitate the proposed development, T15 (two self-seeded sycamore trees positioned outside the site boundary) and TG2 (a small collection of self-seeded sycamore growing along the northern brick wall site boundary) would be removed. It is however noted that these trees are categorised as category C and as such the trees are considered to be of low quality and value and their loss should not constrain the proposed redevelopment of the site. Importantly the mature category A trees to the frontage of the site will be retained.



30. The redevelopment of the site presents a significant opportunity to increase tree cover on site as part of a comprehensive landscape scheme. The applicant has indicated that the remaining trees will be retained and protected throughout the development phase, however an Arboricultural Method Statement and Tree Protection Plan have not been submitted as part of the planning application submission. These details can however be secured via a suitably worded condition.

## **POLLUTION**

31. Core Strategy policy L5.13 indicates that development that causes adverse pollution of air, light, water, ground, noise or vibration will not be permitted unless it can be demonstrated that adequate mitigation measures can be put in place. The following sections of the report provide an assessment of these matters where relevant.

### i) Air

32. The applicant has submitted an Air Quality Screening Assessment and a Construction Dust Assessment in support of the application. The information confirms that the site is not located within an Air Quality Management Area and that the number of vehicle movements associated with the proposed use would not have a significant impact upon air quality. The Construction Dust Assessment report indicates that dust mitigation will be needed during the demolition and construction phases of development for dust mitigation measures to be put in place. A planning condition could therefore be attached to secure the submission of a Dust Management Plan as part of a wider Construction Environmental Management Plan.

### ii) Noise

33. The Council's Pollution and Licencing Officer has recommended that conditions are attached to secure details of the kitchen extraction system and flue for the proposed restaurant, and details of fixed plant machinery to protect the amenity of the nearest noise sensitive premises.

### iii) Contaminated Land

34. NPPF paragraph 121 states that planning decisions should ensure that the proposed site is suitable for its new use taking account of ground conditions, including pollution arising from previous uses and any proposals for mitigation including land remediation or impacts on the natural environment arising from that remediation. The Council's Contaminated Land Officer has reviewed the proposed scheme and identifies that the site is located on brownfield land and as such has recommended that the applicant submits a Preliminary Risk Assessment to assess the potential contamination risks of the site and whether any remediation measures are necessary. These matters can be secured by a suitably worded condition and as such comply with the provisions of Core Strategy policy L5.13.

## **FLOOD RISK AND DRAINAGE**

35. The 0.70 hectare site is located within flood zone 1, however as the site is located within a Critical Drainage Area the applicant has submitted a Flood Risk Assessment in support of the application. The Town and Country Planning Order 2006 defines a Critical Drainage Area as, *“an area within Flood Zone 1 which has critical drainage problems and which has been notified...to...the local planning authority by the Environment Agency.”* The Flood Risk Assessment and Drainage Strategy indicate that there is a low risk of flooding occurring as a consequence of the development.
36. Core Strategy policy L5.18 aims to reduce surface water run off through the use of appropriate measures. The applicant has indicated that surface water and foul sewage will be disposed of via the mains sewer. United Utilities have advised that the site should be drained on a separate system with foul drainage to the public sewer and surface water draining in the most sustainable way. The Council’s Drainage Engineer has reviewed the scheme and has indicated that peak discharge storm water rates should be constrained in accordance with the limits indicated in the Council’s Level 2 Strategic Flood Risk Assessment document. A planning condition is therefore recommended to secure these appropriate discharge rates.

## **SUSTAINABILITY CREDENTIALS**

37. Core Strategy policy L5 requires applicants to demonstrate how major built development proposals will minimise their contribution towards and / or mitigate their effects on climate change. The site is located outside of the Council’s Low Carbon Growth Areas and as such there is a requirement to deliver a carbon dioxide reduction of up to 5% above current Building Regulations. The applicant has submitted a Carbon Budget Statement in support of the application which indicates that the building will be constructed using energy efficient materials and incorporating energy saving technologies that will reduce energy consumption. The incorporation of various renewable technologies into the build design has been considered. Of the technologies considered, the incorporation of photovoltaic cells and air source heat pumps are the most appropriate to achieve on site carbon dioxide reductions. The applicant has estimated that the carbon dioxide savings for the whole site using solar photovoltaic panels is up to 26,800 kgCO<sub>2</sub>/yr.

## **DEVELOPER CONTRIBUTIONS**

38. This proposal is subject to the Community Infrastructure Levy (CIL) and comes under the category of ‘Apartments’, consequently the development will be liable to a CIL charge rate of £0 per square metre in line with Trafford’s CIL charging schedule and revised SPD1: Planning Obligations (2014).
39. No other planning obligations are required.

## CONCLUSIONS

40. The proposed development presents an opportunity to reuse a currently vacant brownfield site for the provision of an extra care facility. The scheme presents an opportunity for future occupiers to buy a property with the benefit of onsite flexible care facilities provided which will allow future occupiers to remain independent and active in older age. The scheme has been assessed and the scheme is considered to be in accordance with the relevant development plan policies. On this basis, it is recommended that planning permission is granted subject to conditions detailed at the end of the report.

## RECOMMENDATION:

### GRANT subject to the following conditions:-

1. Standard time limit 3 years
2. To be development in accordance with approved plans
3. Notwithstanding the provisions of the Town and Country Planning (Use Classes) Order 1987 (as amended) (or any other order revoking and re-enacting that Order), the use of the building and its associated land shall be limited specifically to apartments with care, for those in need of care and shall not be used for any other purpose (including any other purpose within Class C2 of the Schedule to the Town and Country Planning (Use Classes) Order 1987 (as amended) (or any other order revoking and re-enacting that Order).
4. Prior to the first occupation of the development hereby permitted an Operational Management Plan shall be submitted to and approved in writing by the local planning authority. The Operational Management Plan shall be broadly in accordance with the parameters set out within the Planning and Care statements submitted as part of the application, and include details of following:-
  - a) The definition of "Care" for the purposes of the Operational Management Plan;
  - b) The definition of an "Approved Occupier" for the purposes of the Operational Management Plan which shall include the requirements that:
    - (i) such Approved Occupier is aged 60 years or over, and
    - (ii) such Approved Occupier is in need of Care, and
    - (iii) such Approved Occupier enters into a lease substantially in the form of the model form of lease;
  - c) The model form of lease pursuant to which the apartments hereby permitted will be occupied, which shall include provisions to secure that the occupancy of each apartment is restricted to an Approved Occupier throughout the life of the development hereby permitted;
  - d) The procedure for assessing whether an applicant to occupy any apartment hereby permitted is an Approved Occupier; and
  - e) The terms of the occupancy of persons living with an Approved Occupier as part of a single household or a person who was living as part of a single household with an Approved Occupier who has since died.
5. The development hereby approved shall be operated at all times strictly in accordance with the approved Operational Management Plan, unless otherwise first agreed in writing by the Local Planning Authority.

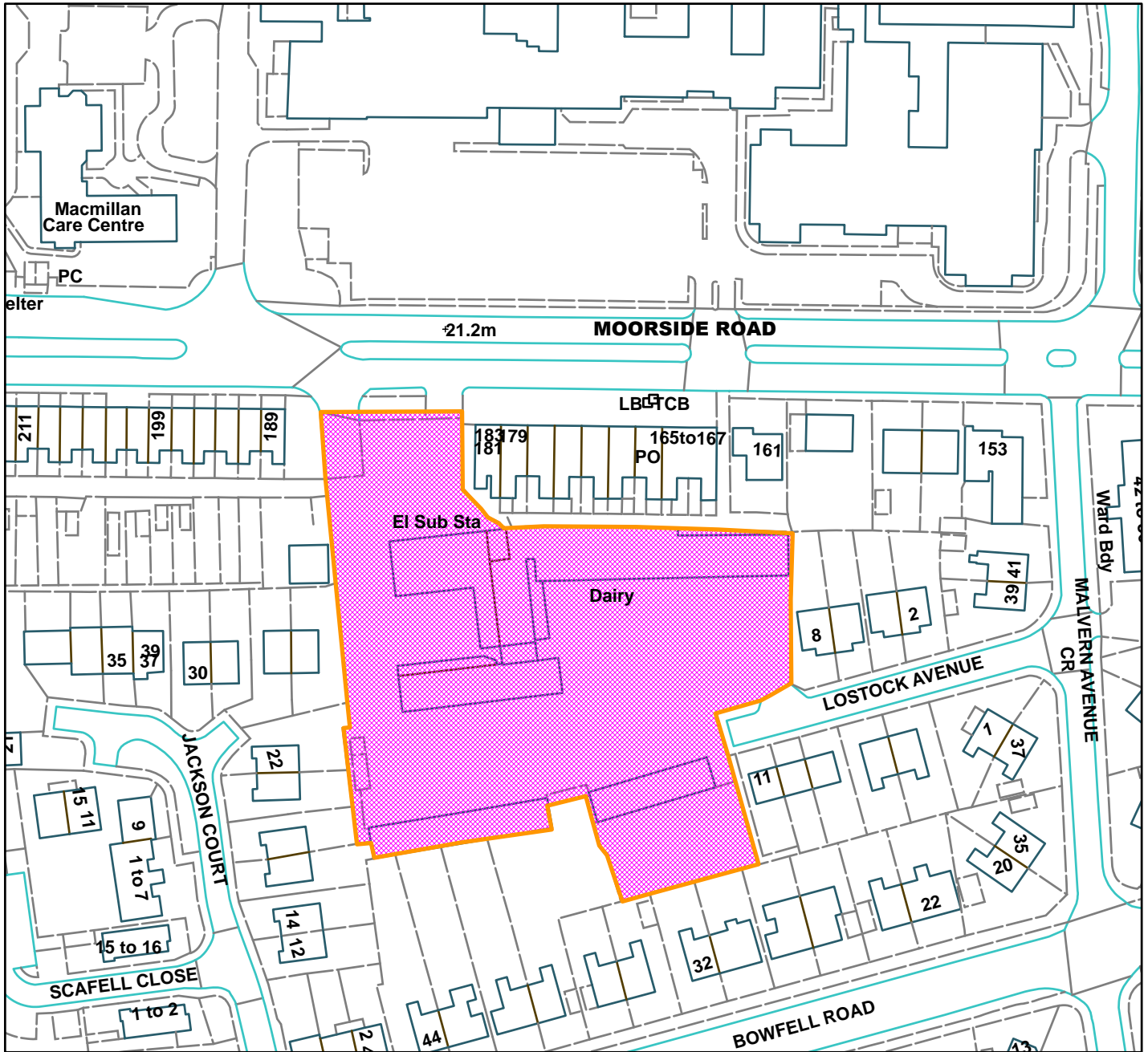
6. Submission and approval of facing materials
7. Landscaping details to be submitted and approved
8. Boundary details to be submitted and approved
9. Landscape maintenance
10. Tree Protection measures
11. Submission of Arboricultural Method Statement
12. Construction Environmental Management Plan
13. External lighting details
14. Fume extraction details
15. Fixed plant machinery
16. Contaminated land
17. Submission of a sustainable drainage scheme to comply with ore Strategy policy L5 and the Strategic Flood Risk Assessment
18. Foul and surface water shall be drained via separate systems
19. Approval of off-site highway works and their implementation prior to first occupation
20. Retention of parking provision
21. Details of cycle provision
22. Travel Plan
23. Provision and retention of balcony screens
24. Obscure glazing
25. Fire service access only from Lostock Avenue

---

**JP**



Former Creamline Dairies, 181 Moorside Road, Flixton (site hatched on plan)



Scale: 1:1,250

Reproduced from the Ordnance Survey map with permission of the Controller of Her Majesty's Stationery Office © Crown Copyright 2012.

Unauthorised reproduction infringes Crown Copyright and may lead to prosecution or civil proceedings.

Organisation	Trafford Council
Department	Planning Service
Comments	Committee date 13/08/2015
Date	03/08/2015
MSA Number	100023172 (2012)

**WARD: Broadheath**

**85296/VAR/15**

**DEPARTURE: YES**

**Variation of condition 17 (Hours of opening and hours of HGV deliveries) of planning permission 79984/FULL/2013 (Demolition of existing warehouse buildings and erection of supermarket (Use Class A1) with car parking, petrol filling station, and associated servicing and landscape works) to allow unrestricted hours for HGV deliveries.**

Asda, Atlantic Street, Altrincham

**APPLICANT:** ASDA Stores Ltd

**AGENT:** Mrs Rebecca Dennis - Pegasus Group

**RECOMMENDATION: MINDED TO GRANT PLANNING PERMISSION SUBJECT TO REFERRAL TO THE SECRETARY OF STATE, SUBJECT TO THE COMPLETION OF A LEGAL AGREEMENT AND SUBJECT TO CONDITIONS.**

---

### **SITE**

The application relates to a 2.8 hectare site located in the Broadheath Industrial Estate. It is some 650m to the west of the junctions of the A56 with George Richards Way and Atlantic Street. The site is bounded by Atlantic Street to the south and George Richards Way to the north with accesses into the site from both roads. The previous warehouse building has been demolished and an ASDA supermarket, car park and associated development approved under planning consent 79984/FULL/2013 is currently under construction. The surrounding area is characterised by a range of business and industrial uses with the Altrincham Business Park situated on the opposite side of Atlantic Street.

### **PROPOSAL**

This application has been made under Section 73 of the Town and Country Planning Act 1990. Section 73 allows applications to be made for permission to carry out a development without complying with a condition(s). It also allows applications to be made to vary condition(s) previously imposed on a planning permission. A Section 73 planning permission is the grant of a new planning consent. However, the original planning permission continues to exist whatever the outcome of the application made under Section 73.

This application seeks to vary condition 17 of planning permission 79984/FULL/2013.

Condition 17 is worded as follows:-

*The retail store hereby approved shall not be open to the public before the hours of 0800 and after 2000 on Sundays. There shall be no HGV service deliveries to the store before the hours of 0800 and after 2000 on Sundays.*

**Reason:** *In the interests of the amenities of the area having regard to Policies L4 and L7 of the Trafford Core Strategy.*

The applicant wishes to amend the condition to remove the restriction on HGV service deliveries before the hours of 0800 and after 2000 on Sundays. The applicant has proposed the following alternative wording as follows:-

*The retail store hereby approved shall not be open to the public before the hours of 0800 and after 2000 on Sundays.*

**Reason:** *In the interests of the amenities of the area having regard to Policies L4 and L7 of the Trafford Core Strategy.*

The development is currently under construction and information has previously been submitted by the applicant to discharge pre commencement conditions. Where this information has been agreed and the condition has been discharged / part discharged, the conditions will be amended on this consent to reflect the position accordingly.

## **THE DEVELOPMENT PLAN IN TRAFFORD**

The Development Plan in Trafford Comprises:

- The **Trafford Core Strategy** adopted 25th January 2012; The Trafford Core Strategy is the first of Trafford's Local Development Framework (LDF) development plan documents to be adopted by the Council; it partially supersedes the Revised Trafford Unitary Development Plan (UDP), see Appendix 5 of the Core Strategy.
- The **Revised Trafford Unitary Development Plan (UDP)**, adopted 19th June 2006; The majority of the policies contained in the Revised Trafford UDP were saved in either September 2007 or December 2008, in accordance with the Planning and Compulsory Purchase Act 2004 until such time that they are superseded by policies within the (LDF). Appendix 5 of the Trafford Core Strategy provides details as to how the Revised UDP is being replaced by Trafford LDF; and
- The **Greater Manchester Joint Waste Plan**, adopted 01 April 2012. On 25<sup>th</sup> January 2012 the Council resolved to adopt and bring into force the GM Joint Waste Plan on 1 April 2012. The GM Joint Waste Plan therefore now forms part of the Development Plan in Trafford and will be used alongside district-specific planning documents for the purpose of determining planning applications.
- The **Greater Manchester Joint Minerals Plan**, adopted 26th April 2012. On the 13th March 2013, the Council resolved that the Minerals Plan, together with consequential changes to the Trafford Policies Map, be adopted and it came into force on the 26th April 2013. The GM Joint Minerals Plan therefore now forms part of the Development Plan in Trafford and will be used

alongside district specific planning documents for the purpose of determining planning applications.

## **PRINCIPAL RELEVANT CORE STRATEGY POLICIES**

L4 – Sustainable Transport and Accessibility

L5 – Climate Change

## **PROPOSALS MAP NOTATION**

E7 - Main Industrial Areas

## **PRINCIPAL RELEVANT UDP POLICIES / PROPOSALS**

E7 – Main Industrial Area

S11 – Development outside established centres

## **NATIONAL PLANNING POLICY FRAMEWORK (NPPF)**

The DCLG published the National Planning Policy Framework (NPPF) on 27 March 2012. The NPPF will be referred to as appropriate in the report.

## **RELEVANT PLANNING HISTORY**

**86278/NMA/15** - Application for a Non-Material Amendment to 79984/FULL/2013 for an additional jet wash bay and landscape amendments. Application approved 3<sup>rd</sup> August 2015.

**84859/NMA/15** - Application for non-material amendment to planning permission 79984/FULL/2013 for new food retail store to allow minor changes to site layout to be in accordance with that approved under planning permission 84088/FULL/2014. Application approved 10<sup>th</sup> March 2015.

**84088/FULL/2014** - Erection of 4no. additional operational facilities ancillary to approved supermarket development including a customer Click and Collect facility, ATM pod, Outdoor Trading Unit, car park maintenance enclosure and associated works to the car park and pedestrian area. Application approved 28<sup>th</sup> January 2015.

**83342/NMA/2014** - Application for non-material amendment to planning permission 79984/NMA/2013 for new food retail store to allow minor changes to main building, car park and external areas, petrol filling station and other changes. Application approved 8<sup>th</sup> September 2014.

**79984/FULL/2013** - Demolition of existing warehouse buildings and erection of supermarket (Use Class A1) with car parking, petrol filling station and associated servicing and landscape works. Application approved 17<sup>th</sup> October 2013.



## **APPLICANT'S SUBMISSION**

The applicant has submitted a Planning Statement and an Acoustic Report in support of the planning application.

## **CONSULTATIONS**

**Local Highway Authority** – There are no significant traffic implications in allowing unrestricted hours of delivery for HGVs. The LHA therefore raises no objections to the application.

**Pollution and Licencing** – The supporting information relating to the above planning application has been reviewed and there are no objections to the above variation which aims to amend the condition to remove the restriction on HGV service deliveries before the hours of 0800 and after 2000 on Sundays.

## **REPRESENTATIONS**

1 letter of representation has been received as a consequence of the planning application publicity. The objector states that ASDA were aware of the condition prior to signing the lease and so should accept it.

## **OBSERVATIONS**

1. Members will be aware that the approval of a Section 73 application grants a new planning permission in its own right. In terms of decision making, regard should be had to any changes on site or in the surrounding area and any changes to planning policy.
2. There have been no significant changes to the site or surrounding area since planning permission was granted, save for the implementation of the permission. The application was determined previously in accordance with the Core Strategy, the saved policies of the Revised Trafford Unitary Development Plan, relevant supplementary planning documents, all of which are still part of the Development Plan for the Borough and the National Planning Policy Framework.
3. The main planning issues considered under the previous application were:-
  - Principle of development
  - Principle of retail development
  - Retail assessment (including consideration of quantitative need, sequential test and impact tests)
  - Impact on town centre vitality and viability, and turnover of the town centres
  - Loss of employment land
  - Accessibility and sustainability of the site
  - Highways
  - Design / layout / materials/landscaping
  - Residential amenity

- Noise
  - Lighting
  - Air quality
  - Ecology
  - Flood risk
  - Contaminated land
  - Developer contributions
  - Off-site highway works
4. No further information has been submitted in support of the planning application, save for the submission of a Planning Statement and Acoustics Report. Information submitted in support of planning application 79984/FULL/2013 remains relevant to the determination of this application.
  5. The matters listed above were considered by Members in the determination of the original application. The scheme is located outside of a defined town centre, is in excess of 5,000 square metres, represents a departure from the development plan and as such was referred to the Secretary of State under Section 77 of the Town and Country Planning Act 1990.
  6. There is no requirement to revisit these issues through the determination of this application. The key issues in the determination of this application relate to the following matters outlined below:-
    - Impacts on the highway network and safety
    - Noise
    - Non material amendments
    - Discharged conditions
    - Developer contributions
    - Referral to Secretary of State

## **IMPACTS ON THE HIGHWAY NETWORK AND SAFETY**

7. The Local Highway Authority has considered the implications of amending the condition to allow unrestricted access to HGV service deliveries to the store and indicate that there would be no significant traffic implications from the proposed variation. On this basis, it is therefore considered, that subject to the conditions detailed at the end of the report, that the scheme complies with Core Strategy policies L4 and L7.

## **NOISE**

8. The applicant has submitted an Acoustic Report in support of the planning application which presents the findings of an environmental noise survey and noise impact assessment for activities and noise sources associated with the proposed development. The Acoustic Report has been reviewed by the Council's Pollution and Licensing officer who has raised no objections to the removal of restriction on HGV service deliveries before the hours of 0800 and

after 2000 on Sundays. On the basis of this advice, it is therefore considered that the variation of this condition is acceptable.

## **NON MATERIAL AMENDMENTS**

9. Since planning permission was granted there have been three Non Material Amendment applications (83342/NMA/2014, 84859/NMA/15 and 86278/NMA/15) submitted and approved by the Council. These amendments relate to plans approved under condition 2 and 7 of the consent and as such these conditions have been updated to reflect the approved amendments. The approval of these applications has no implications for the determination of this proposal.

## **DISCHARGED CONDITIONS**

10. When considering a Section 73 application it is important when considering an application to vary conditions that a Local Planning Authority is mindful of the six tests for the use of planning conditions, i.e. whether they are necessary, relevant to planning and the development to be permitted, enforceable, precise and reasonable in all other respects. When assessing this application to vary condition 17, the Local Planning Authority should take note, in particular, of whether the conditions as currently worded are necessary and reasonable.
11. A number of applications have been submitted to the Council to discharge conditions 4 (surface water drainage details), 5 (underground tanks), 6 (scheme for foul and surface waters), 7 (hard and soft landscaping), 8 (material samples), 10 (recycling), 11 (boundary treatments), 13 (Environmental Management Plan), 14 (contaminated land), 15 (crime prevention measures), 16 (lighting details), 18 (off site highway works), 19 (Traffic Regulation Order), 20 (public transport provision), 21 (parking), 22 (access and servicing), 24 (hackney carriage rank and cycle and pedestrian route), 25 (service yard visibility splays), and 27 (demolition methodology) attached to planning permission 79984/FULL/2013. A number of these conditions have been discharged or part discharged and as such the conditions of the original approval have been amended to reflect the current position. Conditions 11 (boundary treatment details), 12 (vegetation removal during the bird nesting season) and 27 (demolition methodology) of the original consent have been removed as they are no longer necessary or relevant to the consent or had been secured through the implementation of an approved plan.

## **DEVELOPER CONTRIBUTIONS**

12. Granting planning consent under Section 73 of the Town and Country Planning Act 1990, results in the creation of a new planning consent. A S106 Legal Agreement was entered into under the previous application and as such a deed of variation will be entered into to ensure that it relates to this new grant of consent. The applicant and the Council's legal team are currently in the process of preparing a deed of variation.

13. The proposed development would not be liable for Community Infrastructure (CIL) contributions.

## **REFERRAL TO SECRETARY OF STATE**

14. The planning application is a departure from the Development Plan consisting of development outside town centres. Therefore if members are minded to grant the application it will need to be referred to the Secretary of State for their consideration under section 77 of the Town and Country Planning Act and in accordance with the Town and Country Planning (Consultation) (England) Direction 2009.

## **CONCLUSIONS / SUMMARY**

15. The proposed variation of condition 17 of the original consent will allow the approved foodstore to operate in accordance with the operator requirements of the end user without impacting upon having a detrimental impact on matters of noise or highway safety. The wording of the amended condition (condition 15) is outlined below for clarity:-

**“15**

*The retail store hereby approved shall not be open to the public before the hours of 0800 and after 2000 on Sundays.*

**Reason:** *In the interests of the amenities of the area having regard to Policies L4 and L7 of the Trafford Core Strategy.”*

## **RECOMMENDATION:**

### **MINDED TO GRANT PLANNING PERMISSION SUBJECT TO REFERRAL TO THE SECRETARY OF STATE, SUBJECT TO THE COMPLETION OF A LEGAL AGREEMENT AND SUBJECT TO THE FOLLOWING CONDITIONS:**

(A) That the application will propose a satisfactory form of development for the site upon completion of a deed of variation to the legal agreement associated with planning consent 79984/FULL/2013

(B) In the circumstances where the S106 Agreement has not been completed within three months of this resolution, the final determination of the application shall be delegated to the Head of Planning Services; and

(C) That upon satisfactory completion of the above legal agreement, planning permission be GRANTED subject to the following conditions: -

- 1) Standard 3 year time limit
- 2) Approved plans

- 3) The retail store hereby approved shall have a net sales area of no more than 3,754 sq. m. (40,408 sqft) of which no more than 2,628 sq. m (28,288sqft) shall be for the sale of convenience goods.
- 4) Drainage details to be implemented on accordance with the approved surface water drainage scheme as detailed on the Drainage G.A. Plan (05.08.14, reference 13326-D001)
- 5) Development shall be carried out in full accordance with the details of the installation of the underground tanks as detailed on the Drainage G.A. Plan (05.08.14, reference 13326-D001). The scheme shall be fully implemented and subsequently maintained, in accordance with the timing / phasing arrangements embodied within the scheme.
- 6) The disposal of foul and surface waters in accordance with the details on the Drainage G.A. Plan (05.08.14, reference 13326-D001)
- 7) Landscaping details to be carried out in accordance with the hard landscaping details shown plan references 12431-113 Rev B, 13326-D001; and the soft landscaping details shown on drawing reference V12431 L01 Rev J
- 8) The development hereby approved shall be carried out in accordance with the approved materials
- 9) There shall be no external storage of goods, equipment, waste or packing materials or other commercial refuse on or within the customer car parking area or any other area than the service yard.
- 10) The development should not be carried out except in complete accordance with the Waste Recycling, Diversion and Store Operation Processes document
- 11) The development shall be carried out in accordance with the Demolition and Construction Stage Management Method Statement
- 12) The development shall be carried out in accordance with the Geo-Environmental Site Investigation report prepared by REC (reference 45281p3rp, dated July 2014) and the letter from REC (reference 05c45281, dated 1<sup>st</sup> September 2014). The gas membrane shall be validated and a validation statement produced and submitted to, and approved in writing by the Local Planning Authority.
- 13) The development hereby approved shall be carried out in accordance with the crime minimisation measures identified in paragraphs 3.3.6; 3.3.7; 3.3.9 and 4.11 of the Crime Impact Statement (reference 2013/0202/CIS/01 version A, dated 21<sup>st</sup> August 2014) and the boundary treatments identified on the Proposed Site Plan (reference 12431 113 Rev A).
- 14) The development shall be carried out in accordance with the lighting details shown on The External Lighting Luminance Report (drawing reference 10626 E-003 Rev P1, dated September 2014)
- 15) The retail store hereby approved shall not be open to the public before the hours of 0800 and after 2000 on Sundays.
- 16) The development should not be carried out except in complete accordance with the off-site highway works at the junction of the A56 Manchester Road with George Richards Way and at the site access junction with George Richards Way as shown on drawings TPIN1044 – 100 General Arrangement, TPIN1044- 101 Site Clearance, TPIN1044 – 102 Kerbing and Lining Layout, TPIN1044 – 103 Proposed Site Plan - Coded, TPIN1044 – 104 and TPIN1044 105 Setting Out Information by Curtins Consulting Ltd. Development shall not be brought into use until the agreed works have been implemented in full

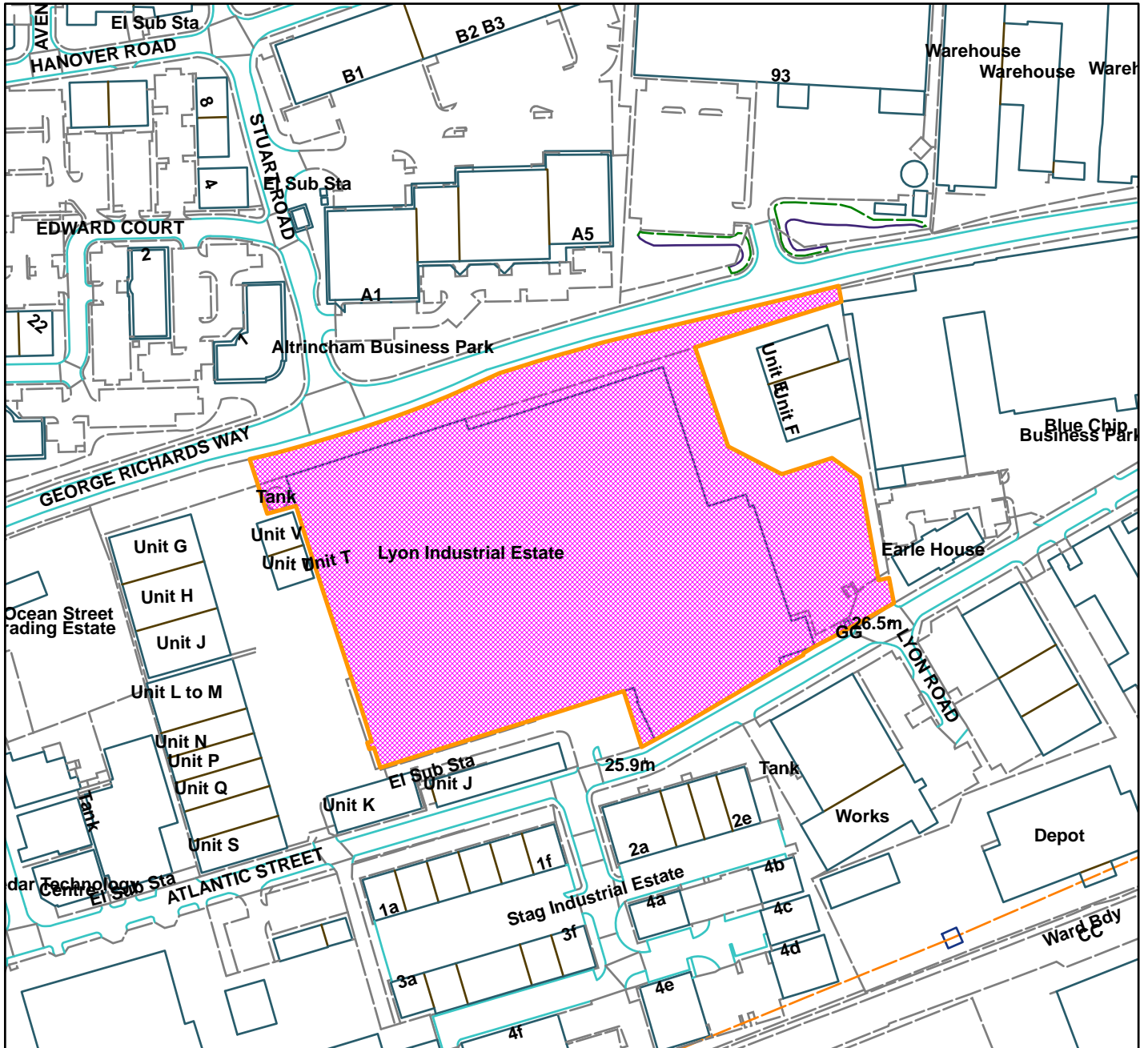
- 17) The development hereby approved, shall be carried out in complete accordance with drawing E8928/02 and the Traffic Regulation Order made operative 10<sup>th</sup> April 2015 for the George Richard Way, Craven Road to A56 section. Development shall not be brought into use until the Traffic Regulation Order has been implemented in full.
- 18) The development shall be carried out in accordance with the public transport provisions detailed within document 90358/PT Provision by Curtins dated 30th January 2015.
- 19) Details of movement, loading, unloading and parking of vehicles/cycles/motorcycles shall be carried out in accordance with the layout shown on Proposed Site Plan 12431 – 101 Rev V and shall be retained thereafter
- 20) The development shall be carried out in accordance with the layout shown on drawing Service Yard Layout 12431-114
- 21) Submission of showers, lockers, changing areas and secure long term cycle parking details
- 22) The development shall be carried out in accordance with the details of the Hackney Carriage Rank and cycle and pedestrian route from George Richards Way as shown on Proposed Site Plan 12431-101 Rev W dated 9<sup>th</sup> April and shall be retained thereafter.
- 23) The development shall be carried out in accordance with drawing TPIN1044-SK011 which provides details of the two shared accesses and the narrowing of the servicing access to improve pedestrian safety along Atlantic Street. The agreed details shall be implemented in full prior to the development first being brought into use and shall be retained thereafter.
- 24) Submission of a Travel Plan within three months of the store becoming operative

---

**JP**



Asda, Atlantic Street, Altrincham (site hatched on plan)



**Scale:** 1:2,500

Reproduced from the Ordnance Survey map with permission of the Controller of Her Majesty's Stationery Office © Crown Copyright 2012.

Unauthorised reproduction infringes Crown Copyright and may lead to prosecution or civil proceedings.

Organisation	Trafford Council
Department	Planning Service
Comments	Committee date 13/08/2015
Date	03/08/2015
MSA Number	100023172 (2012)

**WARD: Clifford**

**85435/FUL/15**

**DEPARTURE: NO**

Erection of 1 x residential apartment building ranging from four to seven storeys, plus basement floor level. Development to provide 106 apartments together with 106 car parking spaces.

**Petrol Station and land adjacent to Chester Road, Old Trafford, M16 9HF**

**APPLICANT:** Kempton Homes

**AGENT:** Mrs Caroline Payne - Emery Planning Partnership Ltd

**RECOMMENDATION: MINDED TO GRANT SUBJECT TO LEGAL AGREEMENT**

---

**SITE**

The application site is located between Chester Road and Stretford Road with a frontage onto both streets and lies approximately 200 metres to the east of the junction at which the two roads converge. To the east, the site fronts onto Nuttall Street, which runs between these two more major roads.

The northern part of the site is occupied by a petrol filling station with four pumps, a shop and a cash machine. The southern part of the site is currently occupied by a hand car wash business, with a canopy and portacabins.

To the north-east, the site lies adjacent to the Empress Conservation Area with large two / three storey red brick Victorian properties fronting onto Chester Road. To the north, on the opposite side of Chester Road, is a row of single-storey industrial units. To the west, the site borders onto an existing seven storey multi-storey car park and, beyond this, lies the sixteen storey West Point office block, which is constructed in red / brown brickwork with blue tinted glass. To the south, on the opposite side of Stretford Road, there are three storey residential flats, constructed in red brickwork and cream render with a grass verge and some trees to the front. To the east, on the opposite side of Nuttall Street, lie commercial premises including an MOT garage and a retail warehouse. Beyond this, further to the east and fronting onto Stretford Road, there are new apartment blocks. These buildings are of up to seven storeys in height and are constructed in grey cladding with white / cream render, red rainscreen cladding and red brickwork.

**PROPOSAL**

Planning permission is sought for the erection of an L-shaped apartment building and basement. The height of the building increases from 4 floors where it fronts onto Chester Road, rising up 7 storeys adjacent to Stretford Road. The building fronts directly onto Stretford Road and Nuttall Street, set behind a small landscape strip, whilst to Chester Road the building is set further back behind a landscaped area. The building will comprise a Newlands blend facing brick (red) with terracotta blockwork between window facings; laminated timber cladding to the first to the fifth



floors (part of); and aluminium cladding panels in a natural finish to part of the fifth and sixth floors.

The scheme will deliver 106 apartments comprising a mix of 66 x 1 bed units and 40 x 2 bed units. There are two main pedestrian accesses provided off Stretford Road and Nuttall Street, whilst vehicular access is provided via the existing priority junction on the A5014 Chester Road. 106 parking spaces, including 9 disability spaces, 6 motorcycle spaces and 146 cycle spaces will be provided on the ground floor and within the basement.

## **THE DEVELOPMENT PLAN IN TRAFFORD**

The Development Plan in Trafford Comprises:

- The **Trafford Core Strategy**, adopted 25th January 2012; The Trafford Core Strategy is the first of Trafford's Local Development Framework (LDF) development plan documents to be adopted by the Council; it partially supersedes the Revised Trafford Unitary Development Plan (UDP), see Appendix 5 of the Core Strategy.
- The **Revised Trafford Unitary Development Plan (UDP)**, adopted 19th June 2006; The majority of the policies contained in the Revised Trafford UDP were saved in either September 2007 or December 2008, in accordance with the Planning and Compulsory Purchase Act 2004 until such time that they are superseded by policies within the (LDF). Appendix 5 of the Trafford Core Strategy provides details as to how the Revised UDP is being replaced by Trafford LDF;
- The **Greater Manchester Joint Waste Plan**, adopted 01 April 2012. On 25<sup>th</sup> January 2012 the Council resolved to adopt and bring into force the GM Joint Waste Plan on 1 April 2012. The GM Joint Waste Plan therefore now forms part of the Development Plan in Trafford and will be used alongside district-specific planning documents for the purpose of determining planning applications; and
- The **Greater Manchester Joint Minerals Plan**, adopted 26th April 2012. On the 13th March 2013, the Council resolved that the Minerals Plan, together with consequential changes to the Trafford Policies Map, be adopted and it came into force on the 26th April 2013. The GM Joint Minerals Plan therefore now forms part of the Development Plan in Trafford and will be used alongside district specific planning documents for the purpose of determining planning applications.

## **PRINCIPAL RELEVANT CORE STRATEGY POLICIES**

- L1 – Land for New Homes
- L2 – Meeting Housing Needs
- L3 – Regeneration and Reducing Inequalities
- L4 – Sustainable Transport and Accessibility
- L7 – Design

L8 – Planning Obligations  
R1 – Historic Environment  
R3 – Green Infrastructure

## **PROPOSALS MAP NOTATION**

Old Trafford Priority Regeneration Area  
Old Trafford Gateway Development Framework  
Adjacent to Empress Conservation Area

## **PRINCIPAL RELEVANT REVISED UDP POLICIES/PROPOSALS**

H10 – Priority Regeneration Area: Old Trafford  
ENV21 – Conservation Areas

## **NATIONAL PLANNING POLICY FRAMEWORK (NPPF)**

The DCLG published the National Planning Policy Framework (NPPF) on 27 March 2012. The NPPF will be referred to as appropriate within the report.

## **RELEVANT PLANNING HISTORY**

**H/69449** - Erection of 1 x seven storey building and 1 x part five, part six storey building for mixed use development comprising 95 apartments, 806 square metres of commercial office space (Use Class B1) and 130 square metres of retail space (Use Class A1) together with 143 car parking spaces. Application approved 24<sup>th</sup> April 2015.

**H/69578** - Change of use to hand car wash and vehicle valeting for temporary period of four years (retrospective). Retention of canopy and site cabin. Application approved 29<sup>th</sup> January 2009

## **APPLICANT'S SUBMISSION**

The applicant has submitted the following documents in support of the planning application:-

- Air Quality Assessment
- CIL Forms
- Crime Impact Statement
- Design and Access Statement
- Drawing and plans
- Draft S106 Heads of Terms
- Environmental Site Assessment
- Groundsure Report
- Heritage Statement
- Planning Statement
- Statement of Community Involvement
- Transport Assessment

- Travel Plan

## **CONSULTATIONS**

**Electricity North West** – No objections.

**GMP Design for Security** – No comments received to date

**Local Highway Authority** – No objection to the proposals on highway grounds provided that 106 car parking spaces are retained and a minimum of 6 motorcycle spaces provided.

**Pollution – Contaminated Land** – No objections subject to a condition requiring the submission of a Preliminary Risk Assessment.

**Pollution & Licensing** – No comments received to date

**Lead Local Flood Authority** – No comments received to date

**United Utilities** – No comments received to date

**Strategic Planning** – No objections. Comments are incorporated within the body of the report

**Transport for Greater Manchester (TFGM)** – The site is well located to take advantage of public transport and active travel alternatives. Cycle parking provision should be provided in accordance with Trafford's Parking Standards and Design SPD.

**Environment Agency** – No objections, subject to conditions to safeguard controlled waters

## **REPRESENTATIONS**

3 letters of objections has been received as a consequence of the planning application publicity. The following issues are identified:-

- Insufficient parking provided within the development which will lead to an increase in on street parking
- Loss of light
- Impact on air quality from increase in cars
- Highway safety
- Loss of employment and local amenity

## **OBSERVATIONS**

### **PRINCIPLE OF DEVELOPMENT**

1. The application site lies within the Old Trafford Priority Regeneration Area as defined by the Unitary Development Plan Proposals Map. Core Strategy

Policy L3 indicates that the Council will support appropriate developments within these areas which will reduce inequalities and secure regeneration benefits, create truly sustainable communities; and make a positive contribution(s) to achieving the Plan's Strategic Objectives and relevant Place Objectives.

2. It is considered that the proposal is consistent with the Regeneration Framework of the Core Strategy and specifically will make a contribution towards achieving Core Strategy Strategic Objectives SO1 (Meeting Housing Needs); SO2 (Regenerate); SO3 (Meet employment need); SO6 (Reduce the need to travel); and SO7 (Secure sustainable development). The proposal will also contribute towards achieving Core Strategy Place Objectives OTO1 (quality, mix and type of residential offer) and OTO2 (maximize the re-use and redevelopment of unused, under used or derelict land).
3. NPPF states that planning should "*encourage the effective use of land by reusing land that has been previously developed (brownfield land), provided that it is not of high environmental value.*" Core Strategy Strategic Objective SO7 seeks to secure sustainable development through promoting the reuse of resources. The proposed development site comprises a petrol station and hand car wash and as such constitutes previously developed land. The development therefore makes effective use of land by re-using land that has been previously developed, whilst protecting the need to release less sequentially preferable or greenfield sites.
4. NPPF paragraph 47 identifies a clear policy objective to, "*boost significantly the supply of housing*". In order to meet future housing need, Core Strategy Policy L1 seeks to release sufficient land to accommodate a minimum of 12,210 new dwellings (net of clearance) over the plan period to 2026. The policy states that this will be achieved through the delivery of new build, conversion and sub division of existing properties.
5. The Council have indicated that it does not, at present, have a five year supply of immediately available housing land, although this site is identified within Trafford's SHLAA (Strategic Housing Land Availability Assessment). The absence of a continuing supply of housing land has significant consequences in terms of the council's ability to contribute towards the government's aim of "*boost(ing) significantly the supply of housing.*" Significant weight should therefore be afforded to the schemes contribution to addressing the identified housing shortfall and meeting the Government's objective of securing a better balance between housing demand and supply, in the determination of this planning application.
6. Core Strategy policy L2.6 indicates that the proposed mix of dwelling types and sizes should contribute to meeting the housing needs of the Borough as set out in the Council's Housing Strategy and Housing Market Assessment. Policy L2.7 states that 1 bed accommodation will, normally, only be acceptable where schemes support the regeneration of Trafford's Town Centres and the Regional Centre.

7. The proposed scheme will deliver the following mix of units:-
- 66 x 1 bed units
  - 40 x 2 bed units
8. The site is neither located within the Regional Centre or within one of Trafford's four town centres, however it is located close to the Regional Centre and within the Old Trafford Regeneration Area. The scheme would help to diversify the mix of housing within the Clifford Ward, which focuses on the delivery of 3 bed properties (2011 census) and will contribute towards a mix of housing stock within this area. It is considered that the scheme will create opportunities for first time buyers; and the private rental sector which given the low turnover of one bedroom accommodation in the social sector will assist those households who are affected by the "bedroom tax" who have to meet their needs in the open market. It is therefore concluded that the proposed mix of units will provide a positive contribution to the housing stock within that area and will contribute towards the creation of a mixed community.
9. Core Strategy Policy L2 indicates that appropriate provision should be made for the provision of affordable housing. The site is located within Old Trafford and as such is classified as being a "cold" market location, where no more than a 5% affordable housing target will be applied under normal market conditions, with a flexibility to raise this to a 10% requirement under "good" conditions. The supporting text of Policy L2 recognises that under "poor" market conditions a 5% contribution could inhibit the delivery of development and therefore where these conditions exist, developments will not trigger a requirement for the provision of affordable housing. The Council's Strategic Planning Team has confirmed that the housing market in Old Trafford is still operating under "poor" market conditions and as such there is no requirement to provide any affordable housing as part of this development.

## **DESIGN, LAYOUT, SCALE AND MASSING**

10. Core Strategy policy L7 states that high quality design is, "*a key element in making places better and delivering environmentally sustainable developments.*" The policy provides policy guidance in respect of design quality, functionality, amenity, security and accessibility.
11. The proposed layout comprises the erection of a single 7 storey residential development block, including basement and ground floor level parking areas. The building is positioned on site to create strong frontages to Nuttall Street and Stretford Road, whilst to Chester Road the building is set back from the building line of nearby buildings, and vehicular access is provided. Indicative landscaping details to the building frontage suggest that there is sufficient scope to provide a visible distinction between the public realm and the curtilage of the building and external elevations help to enliven the streetscene. It is considered that the proposed site layout represents a positive design solution to the site.

12. The principle of a developing a tall building on this site has been previously accepted by the Council as consent was granted for a part 5, 6 and 7 storey building earlier this year. The area surrounding the application site is characterised by a variety of building heights, more specifically 16 storey (West Point), 7 storey (multi storey car park), 4 / 5 storey (Pulse) 3 storey (Bailey Italia / PF Jones), 3 storey (residential block on Chester Road), and 2 storey (properties within the conservation area and light industrial units). Core Strategy policy L7.1 states that development should be appropriate in its context and should enhance the street scene by appropriately addressing matters of scale, height and massing. The applicant has submitted a series of cross sections to demonstrate how the proposed building sits within its immediate context.
13. The height of the building increases from 4 floors where it fronts onto Chester Road, rising up 7 storeys adjacent to Stretford Road. The impact of the development upon the setting of the Conservation Area is discussed below; however in design terms the building is set back and the stepped design ensures that the proposed building does not dominate the buildings within the conservation area and ensures that there is an effective transition between the conservation area and the surrounding area. It is considered that the proposed development would not be detrimental to the character and appearance of the area and the height of the building is considered to be appropriate within its context.
14. The development would provide overlooking and active frontages onto Chester Road, Nuttall Street and Stretford Road in the form of primary and secondary entrances to the apartments and habitable room windows facing towards the street. It is considered that the design and layout of the development would provide for natural surveillance and activity onto Chester Road, Nuttall Street and Stretford Road in accordance with the design principles outlined by Core Strategy Policy L7.
15. The applicant has successfully broken up the mass of the proposed elevations by using a varied palette of materials. The ground floor to (part of) fifth storey element of the proposed building will comprise a Newlands blend facing brick (red) with terracotta block work between window facings (red or grey), whilst part of the fifth and sixth floor of the building will be finished in aluminium cladding (natural finish). The windows comprise powder coated aluminium curtain glazing (anthracite finish) whilst a black powder coated louvre will be incorporated to the western elevation.
16. The proposed scheme will introduce, in part, a new material palette to this part of Old Trafford, however whilst this is different from neighbouring buildings, it is considered that the palette of materials would not detract from the character and appearance of the area and is therefore acceptable. It is recommended that a condition be attached to require the submission of details and samples of all materials for approval.

## **DESIGN AND CRIME**

17. Core Strategy policy L7.4 relates to matters of design and security and states that development must be designed in a way that reduces opportunities for crime. The applicant has submitted a Crime Impact Statement (CIS) in support of the application. The CIS provides an assessment of the scheme against the principles of 'Crime Prevention through Environmental Design' and advises that the scheme should be amended to include secure lobby arrangements and secure boundary treatments to the rear of the building, including the provision of secure vehicular / pedestrian gates. The applicant has revised the scheme accordingly to address the matters raised by the Greater Manchester Police Design for Security team and as such the scheme complies with Core Strategy policy L7, subject to the inclusion of a condition for the scheme to secure appropriate crime prevention measures.

## HIGHWAYS AND PARKING

18. The applicant has submitted a Transport Assessment (TA) in accordance with the requirements of the Framework. The TA demonstrates that the site has a high level of accessibility to a choice of sustainable modes of transport whilst offering future residents, without access to a private car, easy access to a range of schools, employment opportunities, retail and leisure destinations. It is therefore considered that the site comprises a sustainable location for residential development which will enable future occupiers to take advantage of public transport and active travel alternatives.

19. The site will be accessed using the existing priority junction off Chester Road which currently serves the petrol station. Chester Road has a central lane marked out to accommodate vehicles turning right. The TA indicates that the visibility splays of the junction exceeds the policy requirements set out in Manual for Streets. The Local Highway Authority has reviewed the proposed access arrangements and raises no objections.

20. The TA provides an assessment of the impact of the development on the local highway network using information derived from the TRICS database. The applicant concludes that the proposed development for 106 apartments will generate fewer trips than the previously approved scheme for 95 apartments, 806 square metres of office floorspace and a 130 square metre retail unit. On this basis, the applicant concludes that the impacts on the network would be less and as such should be deemed acceptable in terms of highway impacts. The Local Highway Authority has reviewed the TA and raises no objections in terms of the impact on the local highway network.

21. Policy L4.14 and Appendix 3 of the Core Strategy, supplemented by the Parking Standards and Design Supplementary Planning Document (SPD) identify the parking standards for a range of development types across the borough. The SPD identifies three accessibility areas which cover various parts of the Borough. Old Trafford is identified as being located within Area C and as such the following parking and cycle standards apply:-

<b>Dwelling size</b>	<b>Parking spaces</b>	<b>Bicycles</b>
1 bed	1	1 communal or 1

2 – 3 bed	2	allocated 1 communal or 2 allocated
-----------	---	---

22. Paragraph 5.1.2 of the SPD states, *“Where a developer seeks to provide a lower or higher level of parking than the standard set down in the Core Strategy this will need to be fully justified and will need to demonstrate what measures e.g. design, location or operation are to be taken to minimise the need for parking.”*
23. The applicant has submitted a document which, based on data from the Office for National Statistics, indicates that 44.1% of residents in the Clifford ward have no car, 42.7% have 1 car and 13.2% have 2 cars. On the basis of these statistics and applying these to this development, they conclude the scheme would create a demand for 72 car parking spaces. Applying sensitivity testing to these figures, the applicant has predicted likely car ownership in 2025 and predicts that the proposed development would create a demand for car parking of 76 spaces. The applicant therefore concludes that the proposed parking provision is sufficient for the proposed development in this location.
24. The Local Highway Authority has reviewed the submitted information and considers that whilst the number of car parking spaces falls short of the maximum parking standards, they consider that the submitted information provides sufficient justification to deviate from the maximum standards in this instance. The Local Highway Authority therefore do not object to the application on this basis.
25. Core Strategy policy L4.13 states that a Travel Plan should include measures to reduce congestion, improve road safety and promote public transport, walking and cycling as part of an integrated approach to managing travel demand. Although the site is accessible by sustainable modes, it is important to influence travel patterns at the beginning of occupation and therefore the development should be accompanied by a robust Travel Plan. The Travel Plan should feature a range of measures promoting a choice of transport mode, and a clear monitoring regime with agreed targets. The applicant has submitted an interim Travel Plan in support of the planning application which sets out the principles and range of measures which will form part of the final Travel Plan and provides details of its management, implementation and monitoring. It is considered that the submitted document provides a useful starting point for the preparation and submission of a full Travel Plan which can be conditioned as part of this planning permission.

## **AMENITY**

26. There are existing three storey flats at Northumberland Crescent on the opposite side of Stretford Road, which have main habitable room windows facing towards the site of the proposed seven storey block. These flats are approximately 31 metres away from the proposed windows and balconies on the Stretford Road elevation and therefore this relationship would clearly meet the standard in the Council’s Supplementary Planning Guidance, “New



Residential Development". It is possible that there may be some perception of overlooking from the higher storeys of the development. However, given that there is a main road in between and a grass verge with some tree planting in front of the existing flats, it is considered that this would not cause a significant loss of residential amenity.

27. In terms of the relationship to non-residential properties, the existing solicitors' office on the corner of Chester Road and Nuttall Street has three storeys of windows facing the application site at a distance of between approximately 12 metres and 14 metres. However, given that this is only a daytime use and that this is not the main elevation of the office building, it is considered that this relationship would be acceptable.
28. There is a multi-storey car park located to the western boundary of the site which is located approximately 15 metres from the closest habitable room windows. It is therefore considered that there would be no loss of privacy to future occupiers of the development as a result of this relationship.
29. In respect of amenity of future residents, it is considered that adequate light and outlook would be provided from habitable room windows, by virtue of the layout of the building and landscaping. The Council's Guidelines for new residential development indicate 18 sq. m of adequately screened communal area per apartment is generally sufficient for the functional requirements. No outside amenity space for future occupiers would be provided within the development, other than a limited amount of soft landscaping to the front and perimeter of the building. Whilst this could potentially provide some amenity space for occupiers this is well below the above standard and is insufficient for the future occupiers. It is therefore considered a contribution towards off-site provision is justified and this is considered further below.

## HERITAGE

30. Section 12 of the NPPF outlines the government's stance on the determination of applications affecting the setting of designated heritage assets between paragraphs 126 and 141 of the document. Paragraph 126 indicates that Local Planning Authorities should set out in their Local Plan to have, "*a positive strategy for the conservation and enjoyment of the historic environment.*" This is provided by Core Strategy Policy R1 which states that developers must demonstrate how the development will preserve or enhance the Conservation Area and its wider setting. The site lies adjacent to the Empress Conservation Area, designated by the Council under provisions carried forward into Section 69 of the Planning (Listed Buildings and Conservation Areas) Act 1990. The Conservation Area Appraisal prepared for this conservation area is currently in draft form.
31. The applicant has submitted a Heritage Statement (HS) in support of the application which describes the significance of the heritage asset and assesses the impact of the proposal on the significance of the heritage asset. The applicant considers that the development would not result in a detrimental

impact upon the character and appearance of the conservation area and that the development would preserve the character and appearance of the area.

32. The character of the Conservation Area is defined by the rhythm of two substantial terraces of nineteenth century, two storey properties with some additional attic accommodation. The properties are constructed from brick with stone dressings and Welsh slate roof slates. It is considered that the continuous building line formed by the terraces, set back from Chester Road, is an important feature of the streetscene and the Conservation Area.
33. It is considered that the application site and development in the immediate surrounding area detracts from the character and appearance of the Conservation Area. The application site forms an important gateway into the Conservation Area and as such it will have an impact upon the setting of the properties on the south side of Chester Road Conservation Area.
34. The proposed elevation to Chester Road (which is located adjacent to the Conservation Area) will be four storeys in height. The frontage of the proposed building to Chester Road does not project beyond the building line of the terraced properties and is set behind a landscaping strip. It is noted that the proposed scheme is the same height as that which was approved previously under application reference H/69449.
35. Whilst the height of the building is not significantly greater than that of the adjacent terraced properties, it is recognized that the development will block views of the gable end of 497 Chester Road which are currently visible when approaching from the west. It is also recognized that, given the flat roof design, the massing of the proposed building would be significantly greater than the pitched roof terrace properties. Notwithstanding this, it is important to recognize that the application site forms part of the transition between the Conservation Area and the much higher West Point car park and office building to the west. It is considered that any harm to the setting of the Conservation Area, as a result of the massing of the development and the blocking of some views of the adjacent terraces, would be limited. Paragraph 134 of the NPPF states that *“Where a development proposal would lead to less than substantial harm to the significance of a designated heritage asset, this harm should be weighed against the public benefits of the proposal.”*
36. In this case, it is considered that the benefits of the development in terms of additional housing provision in a sustainable location within a Priority Regeneration Area would outweigh any limited harm to the setting of the Conservation Area. It is therefore considered that the development is acceptable in terms of its impact on the Conservation Area, having regard to Policies L7 and R1 of the Trafford Core Strategy and paragraph 134 of the Framework.

## **POLLUTION**

37. The applicant has submitted an Air Quality Assessment (AQA) in support of the planning application. The Council’s Pollution and Licensing Team have

reviewed the AQA and state that the nitrogen dioxide concentrations arising from the operation of the development on existing pollutant levels are negligible. It is recommended that an appropriate construction management plan is implemented to deal with increased dust during the construction phase of development. Subject to the inclusion of the recommended conditions, the proposals are considered acceptable and in accordance with Core Strategy policy L5 and the NPPF.

38. The Council's Contaminated Land Officer has reviewed the proposed scheme and identifies that the site is located on brownfield land and as such has recommended that the applicant submits a Preliminary Risk Assessment to assess the potential contamination risks of the site and whether any remediation measures are necessary. These matters can be secured by a suitably worded condition and as such comply with the provisions of Core Strategy policy L5.13.

39. The Environment Agency (EA) has indicated that the site is located immediately above a Principal and Secondary Aquifer which are classified as being controlled waters. Although the site has been subject to site investigation, the EA consider that this data is out of date and as such they recommend that additional information should be provided prior to the redevelopment of the site to ensure that any contamination issues are fully understood and that the site can be developed safely for residential use. The EA have therefore recommended a series of conditions to ensure that the development does not pose an unacceptable risk to the pollution of controlled waters.

## **FLOOD RISK AND DRAINAGE**

40. Core Strategy policy L5.18 aims to reduce surface water run off through the use of appropriate measures. The applicant has indicated that the surface and foul water will be discharged via the main sewer. A planning condition has been recommended to ensure peak discharge storm water rates are constrained in accordance with the limits indicated in the Council's Level 2 Strategic Flood Risk Assessment document and that surface water drainage is passed through an oil interceptor.

## **DEVELOPER CONTRIBUTIONS**

41. This proposal is subject to the Community Infrastructure Levy (CIL) and comes under the category of 'Apartments' (cold charging zone) and consequently the development will be liable to a CIL charge rate of £0 per square metre in line with Trafford's CIL charging schedule and revised SPD1: Planning Obligations (2014).

42. In accordance with Policy L8 of the Trafford Core Strategy and revised SPD1: Planning Obligations (2014) it is necessary to provide an element of specific green infrastructure. SPD1: Planning Obligations sets out a standard of 1 tree per apartment and therefore this development would be expected to provide 106 trees on site. Due to the footprint of the proposed development there is

only limited scope for tree planting on site. The submitted plans indicatively identify some tree planting to the Chester Road frontage, however clearly it would not be possible to provide the required quantum of planting to satisfy this policy requirement. The SPD states the provision of alternative Green Infrastructure treatments could be provided in lieu of, or in combination with, tree provision. In this case, this may include scope for the provision of native tree and hedge species, a green roof or green wall. The requirement to provide Green Infrastructure treatment on site and to include details of the tree planting and landscaping at the front of the site as indicated on the site plan, can be secured by condition.

43. Policy L7 requires development to make appropriate provision for open space, where appropriate, in accordance with Policy R5. This policy requires all development to contribute, on an appropriate scale, to the provision of the standards set out in the policy either by way of on-site provision, off site provision or by way of a financial contribution towards improving quantity or quality of provision.

44. The Planning Obligations SPD states that large residential developments of approximately 100 units will need to provide new open space as part of the site design, however the development provides only limited amenity space to the front of the building. The development will have an impact upon the use of local parks and as such a contribution towards Hullard Park would be applicable and would meet the necessary tests outlined in the NPPF and CIL regulation 123. The matters will be secured via a S106 Legal Agreement. Using the calculations provided by the Planning Obligations SPD a contribution of £52,653 would be payable, calculated as follows:-

- 66 x 1 bed apartments equates to 85 residents requiring LOS at £161.59 per person = £13,735
- 40 x 2 bed apartments equates to 72 residents requiring LOS at £161.59 per person = £11,634
- 40 x 2 bed apartments require play provision at £378.95 per person which = £27,284

## CONCLUSIONS

45. Paragraph 17 of the Framework indicates that, *"every effort should be made objectively to identify and then meet the housing....needs of an area"* and further that Local Planning Authorities *"boost significantly the supply of housing"* (paragraph 47). Granting planning consent for residential development on this site will make an important contribution towards addressing the housing supply shortfall within the Borough within a Priority Regeneration Area. The adverse impacts of the proposed development have been considered and assessed and it is considered that impacts associated with the proposed development would not demonstrably and significantly outweigh the benefits of the scheme as required by NPPF paragraph 14. Other development impacts associated with the scheme can be mitigated through the use of planning conditions, Section 106 and S278 Agreements where necessary. In accordance with paragraph 7 of the Framework, it is

considered that the proposed development represents a sustainable form of development, when considered against the relevant policies of the NPPF which would deliver significant benefits, including primarily the delivery of housing. It is therefore recommended that planning permission should be granted subject to conditions and entering into a Section 106 Legal Agreement.

## **RECOMMENDATION**

### **MINDED TO GRANT SUBJECT TO LEGAL AGREEMENT and subject to the following conditions:**

(A) That the application will propose a satisfactory form of development for the site upon completion of an appropriate legal agreement to secure a maximum financial contribution of £52,653 towards improvements towards schemes to provide a wildlife pond area, biodiversity and inclusive access improvements and play area enhancements at Hullard Park; and

(B) In the circumstances where the S106 Agreement has not been completed within three months of the resolution to grant planning permission, the final determination of the application shall be delegated to the Head of Planning Services; and

(C) That upon satisfactory completion of the above legal agreement, planning permission be GRANTED subject to the following conditions: -

1. Time limit 3 years
2. List of approved plans
3. Details of materials to be submitted and approved
4. Landscaping details to be submitted and approved including green infrastructure
5. Details of landscape maintenance for a minimum period of 5 years
6. No development shall commence until a Crime Prevention Plan to include measures to reduce opportunities for crime has been submitted to and approved in writing by the Local Planning Authority. The development shall only be carried out in accordance with these approved details.
7. Details of gate / shutter to car park and underground basement parking to be submitted and approved
8. Approved access, parking and turning details
9. Access, parking and turning areas to be available at all times
10. Submission, approval and implementation of a full Travel Plan
11. Provision of cycle storage facilities
12. Contaminated land
13. Submission of external lighting details.
14. Submission of report detailing the impact on TV reception, mitigation measures and complaints procedure.
15. Restrict piling or any other foundation works subject to demonstrating that the activities would not result in an impact upon groundwater
16. Existing and finished site levels
17. Existing and finished floor levels
18. Surface water drainage to be passed through an oil interceptor

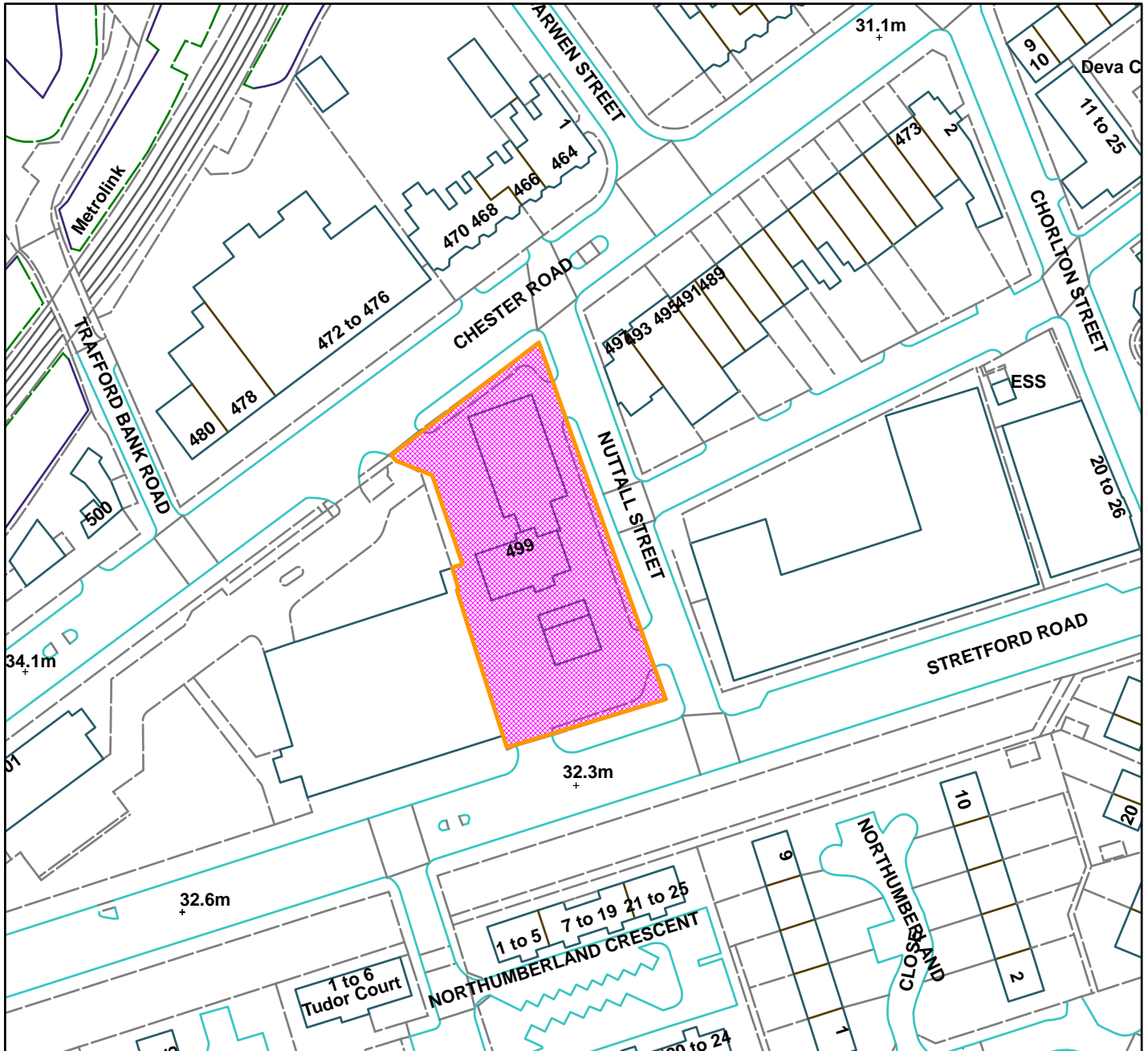
19. Submission of drainage details to comply with ore Strategy policy L5 and the Strategic Flood Risk Assessment
20. Submission, approval and implementation of a Construction Environmental Management Plan

---

**JP**



Petrol Station & land adjacent to 499 Chester Road, Old Trafford (site hatched on plan)



**Scale:** 1:1,250

Reproduced from the Ordnance Survey map with permission of the Controller of Her Majesty's Stationery Office © Crown Copyright 2012.

Unauthorised reproduction infringes Crown Copyright and may lead to prosecution or civil proceedings.

Organisation	Trafford Council
Department	Planning Service
Comments	Committee date 13/08/2015
Date	03/08/2015
MSA Number	100023172 (2012)

**WARD:** Bowdon

**85511/HHA/15**

**DEPARTURE: No**

**Erection of single storey rear extension and erection of front porch.**

18 Primrose Cottages, Brickkiln Row, Bowdon, WA14 3EL

**APPLICANT:** Mr Maddocks

**AGENT:** Tsiantar Architects Ltd

**RECOMMENDATION: GRANT**

---

**Councillor Hyman has requested that this application be determined by the Planning Development Control Committee for reasons set out within the report**

**SITE**

The application site comprises a two storey mid-terraced dwelling built in 1890 with its northern elevation fronting onto Brickkiln Row and its southern elevation backing onto a large, informal recreational area that is shared by the linked properties and accessed from the public highway via paths to either side of the row.

The immediate area is characterised by terraced housing with a variety of semi-detached and detached properties of different periods within the wider residential area. The properties themselves are relatively small, with the two bedroom application property still having its original kitchen on its northern side which is cramped in its form and of a minimum size.

No.18 currently has a small sun room to its southern elevation similar in its projection and lean-to roof design to its neighbours who have similar single storey brick extensions and raised terraced areas of various designs with access steps down to either a small yard area or the pathway that allows informal access across the entire width of Brickkiln Row.

The area to the rear of these properties is relatively private with boundary fencing and mature planting providing a relaxed and informal recreational area for residents.

The application property and the entire row are not currently within a Conservation Area or within the setting of a Listed Building. Whilst the recently produced draft Bowdon Conservation Area Appraisal is currently out to Public Consultation, it does not propose the boundary to extend to include the properties on Brickkiln Row.

**PROPOSAL**

The applicant proposes the demolition of an existing sun room and the erection of a single storey rear extension that would provide increased accommodation on a single



level with internal steps allowing for a projection of 4.275m from the main southern elevation (2.8m further than both adjacent single storey projections) with a mono-pitched roof with its maximum height being similar to the existing sun lounge and eaves being reduced by 800mm when compared to the existing structure. Due to the angle of roof proposed, natural slate tiles would not be able to be accommodated and therefore a zinc material with similar colour to natural slate is proposed. Within the southern elevation, a secondary apex roof is proposed with a glazed gable above bi-folding doors which have been inset by 500mm. The side elevations of the proposed development would be Multi-Red Cheshire commons.

Within the northern elevation a storm porch is proposed with a width of 1737mm and depth of 1200mm together with associated block paving.

The increase in floor space of the proposed development would be 12.66m<sup>2</sup>.

## **DEVELOPMENT PLAN**

### **The Development Plan in Trafford Comprises:**

- The **Trafford Core Strategy**, adopted 25th January 2012; The Trafford Core Strategy is the first of Trafford's Local Development Framework (LDF) development plan documents to be adopted by the Council; it partially supersedes the Revised Trafford Unitary Development Plan (UDP), see Appendix 5 of the Core Strategy.
- The **Revised Trafford Unitary Development Plan (UDP)**, adopted 19th June 2006; The majority of the policies contained in the Revised Trafford UDP were saved in either September 2007 or December 2008, in accordance with the Planning and Compulsory Purchase Act 2004 until such time that they are superseded by policies within the (LDF). Appendix 5 of the Trafford Core Strategy provides details as to how the Revised UDP is being replaced by Trafford LDF.
- The **Greater Manchester Joint Waste Plan**, adopted 1<sup>st</sup> April 2012 now forms part of the Development Plan in Trafford and will be used alongside district-specific planning documents for the purpose of determining planning applications.
- The **Greater Manchester Joint Minerals Plan**, adopted 26<sup>th</sup> April 2013 now forms part of the Development Plan in Trafford and will be used alongside district-specific planning documents for the purpose of determining planning applications.

### **PRINCIPAL RELEVANT CORE STRATEGY POLICIES**

L4 – Sustainable Transport and Accessibility

L7 – Design

### **PROPOSALS MAP NOTATION**

None.

### **PRINCIPAL RELEVANT REVISED UDP POLICIES/PROPOSALS**

None

## **NATIONAL PLANNING POLICY FRAMEWORK (NPPF)**

The DCLG published the National Planning Policy Framework (NPPF) on 27 March 2012. The NPPF will be referred to as appropriate in the report.

## **RELEVANT PLANNING HISTORY**

85045/HHA/15 – Demolition of existing car port to the front of the dwelling, reduction in height of existing boundary wall, addition of timber trellis and the erection of a new timber fence with vehicular and pedestrian access to the front boundary. Approved with conditions 1<sup>st</sup> May 2015.

85094/PAH/15 - Erection of a single storey rear extension with a maximum projection of 5.945 metres beyond the original rear wall, a maximum height of 3.653 metres and eaves height of 3 metres. Application for prior approval under part 1 of schedule 2 class A of the Town and Country Planning (General Permitted Development) Order 1995 (as amended). Prior approval required and refused.

## **APPLICANT'S SUBMISSION**

The applicant has submitted a Design Statement in support of the proposed development.

## **CONSULTATIONS**

None

## **REPRESENTATIONS**

**Councillor Hyman** has called in the application for consideration by Planning Committee in response to concerns raised by a number of residents within Primrose Cottages, relating to the proposed development being overdominant, being intrusive to the open view across the southern elevation of the row of dwellings currently provided, and not being in keeping with the nature of the row of properties.

**Neighbours:** 5 objections received based upon initial scheme – summarised below :

- South elevation of property is not the rear but the principle elevation
- Primrose Cottages are unique and it is the Council's duty to protect their individuality
- Single storey rear extensions should not project more than 3m from the (rear) elevation
- Gross overdevelopment of plot – a monstrosity
- Loss of privacy by the ability of potentially walking passed the bi-folding doors on right of way to rear of properties
- Design of extension is poor and unsympathetic and fails completely to complement the existing character of Primrose Cottages – the tradition of the terrace should not be obscured in any way

- Development would cause an overbearing effect, curtail the amount of sunlight currently received by 16 and 20 Primrose Cottages and harm the open aspect currently enjoyed to open plan rear garden areas
- All other extensions have been completed in a similar design, height and length to each other – proposed extension should be in keeping with these
- Previous Prior Notification application was refused due to “overbuild” – this application provides only a minimal change and the loss of views would affect sense of security and community inclusion

1 email of support received:

- Occupier of No.2 states that there would be no impact by the proposed development.

3 further comments received following amended plans re-consulted:

- The cottage fronts out onto the long garden, the back yards, as when they were built were entered from the street
- No variation of previous objections

## **OBSERVATIONS**

### PRINCIPLE

1. Although its age and location is noted as being part of a row of properties that historically was used by laundry workers on behalf of properties along Green Walk (hence the long and open communal garden area for the drying of clothes) the property is not considered to be of sufficient architectural or historical value to be designated as a non-designated heritage asset. The properties are not within the Bowdon conservation area nor are they currently proposed for inclusion in the Bowdon conservation area.
2. It is therefore considered that any alterations to the dwelling are supported in principle, subject to being of a suitable siting, size and design without causing detrimental harm to neighbouring residents.

### DESIGN AND APPEARANCE

3. One of the 12 core planning principles of the NPPF is to always seek to secure high quality design and a good standard of amenity for all existing and future occupants of land and buildings (paragraph 17). Paragraph 56 of the NPPF states that the Government attaches great importance to the design of the built environment - good design is a key aspect of sustainable development, is indivisible from good planning, and should contribute positively to making places better for people. Paragraph 64 of the NPPF states that permission should be refused for development of poor design that fails to take the opportunities

available for improving the character and quality of an area and the way it functions.

4. In relation to matters of design, Policy L7 of the Core Strategy states development must:
  - Be appropriate in its context;
  - Make best use of opportunities to improve the character and quality of an area;
  - Enhance the street scene or character of the area by appropriately addressing scale, density, height, massing, layout, elevation treatment, materials, hard and soft landscaping works, boundary treatment.
5. SPD 4: A Guide for Designing House Extensions and Alterations recommends that extensions to reflect the character, scale and form of the original dwelling by matching and harmonising with the existing architectural style and detailing. The SPD sets out specific guidance relating to these areas.
6. Section 2.2.3 of the above guidance states that “A contemporary design or approach may be acceptable however such proposals must be justified and explained in detail as to why the individual proposal is appropriate, responsive to the character of the property and the surrounding context. This type of submission needs to demonstrate high quality design and a clear design process that is thorough, well thought-through and carefully responsive to its context”.
7. The design of the most recently amended development is considered to be of a good standard with reasoning adequately provided within the updated Design Statement.
8. The proposed details show that the extension would be set at a lower level than the main dwelling, with its southern elevation being glazed with a gabled design to allow unrestricted views to the shared amenity area without being overdominant to the host building or to the row of terraced properties known as Primrose Cottages to which it would relate. The scale and massing of the design is considered to be appropriate and the inwardly opening set of bi-folding doors would add further character and allow for the doors to open without impeding upon the access path to its rear, whilst the proposed roof material “Zink tata Steel mono pitch roof would have a similar colouring to natural slate.
9. Furthermore, the siting of the proposed porch extension on the northern elevation is considered not to be visually intrusive and would not detract from the character of the streetscene. Therefore this element and the removal of a dated sunroom within the southern elevation and replacement with the proposed extension are recommended to be acceptable in terms of design and comply with Policy L7 of the Trafford Core Strategy in this respect.

## RESIDENTIAL AMENITY

10. One of the 12 core planning principles of the NPPF is to always seek to secure high quality design and a good standard of amenity for all existing and future occupants of land and buildings (paragraph 17).
11. Policy L7 of the Core Strategy states that in relation to matters of amenity protection development must not prejudice the amenity of future occupiers of the development and/or occupants of adjacent properties by reason of overbearing, overshadowing, overlooking, visual intrusion, noise or disturbance, odour or in any other way.
12. The development would project 4275mm from the main southern elevation and 2.8m further than the existing single storey extensions of both 16 and 20 Primrose Cottages. The Council's guidance within SPD4: A Guide for Designing House Extensions & Alterations indicates that a projection of up to 3m further than an adjacent habitable room would be reasonable and this also contained within the Government's guidance regarding standard permitted development rights – the proposed projection is therefore less than this recommended distance and considered reasonable.
13. The eaves height from external floor level is measured as being 2625mm and would stand approximately 1800mm higher than the floor level of both neighbouring habitable rooms, with the roof increasing in height moving away from the boundary to allow for minimal impact upon the current views enjoyed by the occupiers of adjacent properties, with no side openings or windows proposed to protect amenity. The maximum height of the extension would be similar to existing extensions within Brickkiln Row.
14. The existing sunroom to the rear of the property allows for some overlooking of neighbouring yard areas directly to the rear of neighbouring properties, whereas the proposed development would have its opening level with the ground level thereby reducing this overlooking opportunity to the benefit of residential amenity. The current openness to the south of this row of properties is unusual in that there is a lack of privacy with no boundary fencing or planting to segregate dwellings. The proposed development, although larger than existing extensions and differing in its design is considered to be compatible with the character of these cottages whilst not being unduly dominant or excessive in its size, scale or massing.
15. The proposal is therefore considered to be in accordance with policy L7 of the Trafford Core Strategy and the thrust of the NPPF as it would not adversely affect the level of residential amenity neighbouring residents can reasonably expect to enjoy and the development would provide future occupants with a satisfactory standard of amenity.

## PARKING AND HIGHWAY SAFETY

16. Policy L4 of the Trafford Core Strategy which relates to sustainable transport and accessibility, seeks to ensure that all new developments do not adversely affect highway safety, with each development being provided with adequate on-site parking in line with the maximum standards set out in appendix 3. The current property has two bedrooms and the proposed porch within the northern elevation has been reduced by 300mm to project 1200mm from the main wall so as not to impact upon existing off-street car parking provision.

## CONCLUSION

17. The proposed extension would be acceptable in terms of visual and residential amenity and would comply with the National Planning Policy Framework and more localised considerations such as the Council's Core Strategy and Supplementary Planning Guidance regarding house extensions. It is therefore recommended that planning permission should be granted, subject to conditions.

## CIL

The increase in floor space of the proposed development would be 12.66m<sup>2</sup> and would therefore not attract any Community Infrastructure Levy (CIL).

## **RECOMMENDATION: GRANT subject to the following conditions:-**

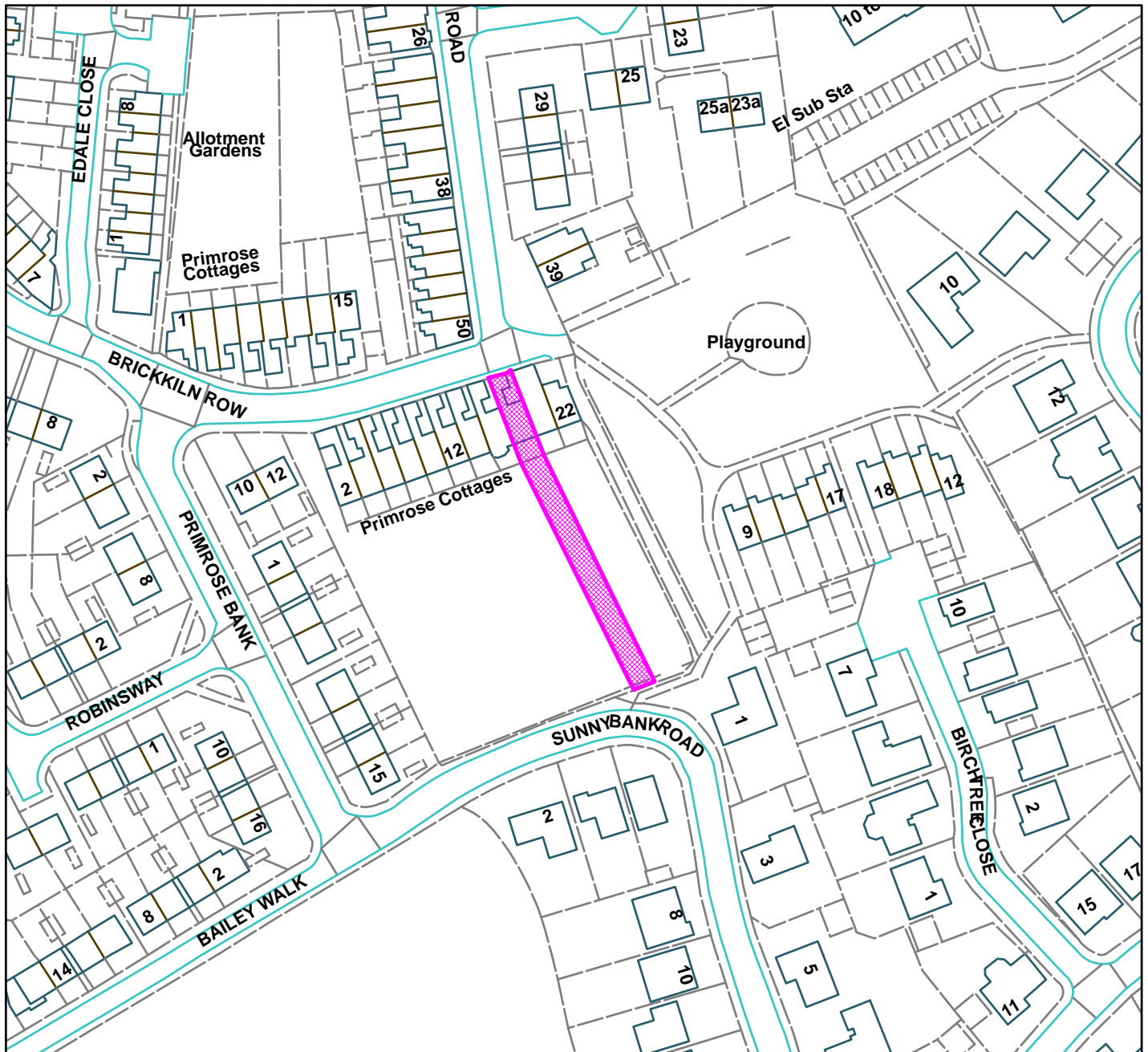
1. Standard
2. Compliance with all plans
3. Materials to be submitted
4. No further openings to side elevations

GD

---



18 Primrose Cottages, Brickkiln Row, Bowdon (site hatched on plan)



Scale: 1:1,250

Reproduced from the Ordnance Survey map with permission of the Controller of Her Majesty's Stationery Office © Crown Copyright 2012.

Unauthorised reproduction infringes Crown Copyright and may lead to prosecution or civil proceedings.

Organisation	Trafford Council
Department	Planning Service
Comments	Committee date 13/08/2015
Date	03/08/2015
MSA Number	100023172 (2012)

**WARD:** Bowdon

**85655/FUL/15**

**DEPARTURE: No**

**Erection of detached SIBCAS (Modular building) structure for use as a children's day nursery with associated works thereto.**

Bowdon Church Of England Primary School, Grange Road, Bowdon, WA14 3EX

**APPLICANT:** Bowdon Church of England School

**AGENT:** Bowker Sadler Architecture

**RECOMMENDATION: REFUSE**

---

**SITE**

The application site comprises a primary school and is located on the east side of Grange Road. The existing school building is a single storey building which has been extended previously, the school building is located close to the northern boundary of the site.

The site is currently undergoing redevelopment following planning approval in August 2014 for the erection of a new school building. The building is almost complete with an anticipated opening for the new September term. The existing school building will be demolished as part of the redevelopment of the site.

To the north side of the site is a public footpath leading to York Drive, beyond the footpath to the north side is the Bowdon Cricket, Hockey & Squash Club. To the east side of the site are residential properties on Theobald Road and York Drive; to the south side of the site is public recreational land, beyond which is the Lady of the Vale nursing home. Grange Road is located to the west side of the site leading to a number of residential side roads such as Fletcher Drive, Weaver Close, Thatcher Close and York Road.

**PROPOSAL**

This planning application seeks approval for the formation of a pre-school age nursery within the school site utilising an existing SIBCAS building (mobile classroom). The existing mobile classroom was previously used for the before and after school club for the primary school children prior to the redevelopment of the school. It is now proposed to have the before/after school club within the new school building and the mobile classroom will be used for the nursery.



As part of the approved redevelopment of the site, the mobile classroom was proposed to be relocated from its position near to the western boundary of the site over towards the eastern side of the site. This current proposal will involve the mobile classroom being situated in the same position towards the eastern side of the site as approved on the wider redevelopment of the site.

The nursery will have 22 places for children aged 3 and 4 (age 3 by 31 August 2015 for the 2015/16 intake). Three full time and one part time member of staff would be employed to operate the nursery. The nursery day would start at 08.30am and ends at 15.00pm during school term time only. The nursery would be operated by Bowdon Preschool, a local private sector early years education provider who are currently located at St Lukes Church Hall, Bowdon Vale and also use the parish centre, but will relocate to the proposal site if planning permission is granted. The Bowdon Preschool Company promotes Christian values and has become a feeder nursery to Bowdon Church school.

Pedestrian access for the pre-school nursery will be via the pedestrian path that extends across the entire northern boundary of the school site.

## **DEVELOPMENT PLAN**

### **The Development Plan in Trafford Comprises:**

- The **Trafford Core Strategy**, adopted 25th January 2012; The Trafford Core Strategy is the first of Trafford's Local Development Framework (LDF) development plan documents to be adopted by the Council; it partially supersedes the Revised Trafford Unitary Development Plan (UDP), see Appendix 5 of the Core Strategy.
- The **Revised Trafford Unitary Development Plan (UDP)**, adopted 19th June 2006; The majority of the policies contained in the Revised Trafford UDP were saved in either September 2007 or December 2008, in accordance with the Planning and Compulsory Purchase Act 2004 until such time that they are superseded by policies within the (LDF). Appendix 5 of the Trafford Core Strategy provides details as to how the Revised UDP is being replaced by Trafford LDF.
- The **Greater Manchester Joint Waste Plan**, adopted 1<sup>st</sup> April 2012 now forms part of the Development Plan in Trafford and will be used alongside district-specific planning documents for the purpose of determining planning applications.
- The **Greater Manchester Joint Minerals Plan**, adopted 26<sup>th</sup> April 2013 now forms part of the Development Plan in Trafford and will be used alongside district-specific planning documents for the purpose of determining planning applications.

### **PRINCIPAL RELEVANT CORE STRATEGY POLICIES**

L4 – Sustainable Transport and Accessibility

L5 – Climate Change

L7 – Design

L8 – Planning Obligations

R2 – Natural Environment  
R3 – Green Infrastructure  
R4 – Green Belt, Countryside and Other Protected Open Land  
R5 – Open Space, Sport and Recreation

## **PROPOSALS MAP NOTATION**

Protected Open Space

## **PRINCIPAL RELEVANT REVISED UDP POLICIES/PROPOSALS**

OSR5 – Protection of Open space

## **NATIONAL PLANNING POLICY FRAMEWORK (NPPF)**

The DCLG published the National Planning Policy Framework (NPPF) on 27 March 2012. The NPPF will be referred to as appropriate in the report.

## **RELEVANT PLANNING HISTORY**

82725/FULL/14 - Erection of new two storey school building incorporating new carpark (including the erection of floodlighting columns), cycle & scooter parking, internal access road with drop off area and formation of new vehicular access with associated security barriers. Provision of new multi-use games area (muga) and all weather pitch with retention and realignment of existing 2 x grass pitches, cricket wickets and running track area and provision of new junior grass pitch. Provision of new bin storage area, relocation of existing before and after school building and relocation of ancillary storage structures/storage containers. Associated landscaping throughout including provision of new security fencing. Demolition of existing school building. – Approved August 2014

## **APPLICANTS SUBMISSION**

The applicant has submitted a Design & Access Statement in support of the proposal, details of which will be referred to as necessary within this report.

## **CONSULTATIONS**

**Local Highway Authority (LHA)** – Object, for the following reason:-

The proposed children's day nursery will accommodate up to 22 children and there will be three full time staff and a part time staff member also. Three car parking spaces are proposed for use by staff, but no parent parking will be provided within the application site or at the school. The Primary School will also manage the school pick-up/ drop-off area adjacent to Grange Road to prevent any parent parking for either the school or the proposed day nursery. The age of the children attending the day nursery will require them to be accompanied by a parent or other authorised adult to and from the day nursery building via the public footpath, which provides access to the site from both Grange Road and from York Drive.

It is expected that some children will be walked to and from the day nursery from the immediate area, but in view of the age of the children and the likely catchment area it is considered likely that the majority of children will be transported to and from the site by car. Although some children at the day nursery will have siblings at the adjacent school, many will not and the day nursery will therefore generate additional car trips and additional demand for parking on Grange Road, York Drive, York Road etc. These roads are already extremely busy at school times when high levels of on-street parking occur on roads within close walking distance of Bowdon CofE Primary School, and although the school has a Travel Plan in place this situation is likely to continue. The hours of opening of the day nursery are similar to those of the Primary School therefore many of the car trips and parking demand generated by the proposed day nursery use will be occurring at similar times to the busy school times when neighbouring roads are extremely busy, resulting in additional disamenity and inconvenience to local residents.

### **Pollution, Housing & Licencing – No objections**

**Education and Early Years (Trafford Council)** - The Education and Early Years Commissioning Service has a neutral stance concerning this planning application. The LA has not provided any funding either to erect the modular building or to help with the transfer of an existing early years provider to these premises should the planning application be approved. Nor would we object to the planning application.

The LA will only financially support the creation of childcare and early education where there is insufficient provision to meet the statutory duty under the Childcare Act 2006. The School approached the LA in 2012 for funding for a new maintained nursery class but as there was sufficient provision within the local area it was decided not to allocate any capital funding.

However there is a draft Childcare Bill before the House of Lords setting out the Government's intention to extend the current free entitlement for 3 and 4 year olds, with working parents, from 15 to 30 hours of free childcare per week for 38 weeks by September 2017. These changes will impact significantly on schools and the private sector as it is likely that there will be insufficient places available for all children who qualify. The Small Business, Employment and Enterprise Act 2015 has allowed schools to lower their age range for children aged 2. These legislative and policy changes are intended to encourage and make it as easy as possible for schools to offer childcare themselves, commission a private provider or hire rooms to private providers to deliver childcare.

### **REPRESENTATIONS**

**Neighbours** – 36 letters of objection have been received regarding the proposal citing the following concerns:-

## Highways

- If approved this proposal will add to already considerable traffic problems which arise due to lack of consideration by parents of Bowdon Church School towards residents.
- Parking and access situation will worsen when the school capacity increases by 50%.
- The new school is in the process of being built, no real assessment of additional increase as a result of this proposal, decision should be delayed until school is up and running.
- School children must always be brought into the school; this will require the parents having to park.
- Additional traffic to the area following Bowdon Cricket Club no longer allowing parents to use their parking facilities (and why should they)
- Lives put at risk if child stumbles into road, ambulances and fire engines unable to get down Grange Rd because of congestion
- Traffic congestion incidents have been reported to the police
- Parents park on double yellow lines, block residents driveways
- Pedestrians have to use road as pavements blocked by parking
- Restricted visibility for pedestrians due to parked vehicles
- Raised traffic hump at junction of York Drive and Grange Rd often not observed by large vehicles which bounce over them
- Double yellow lines on Grange Rd are a nuisance and make York Rd very difficult to negotiate as cars are parked on pavements - also an inconvenience to residents on Grange Rd when tradesmen are on site.
- Vibration from traffic causing damage to residential properties
- Parent's handbook on school web-site states that there is no parking for parents at the school site and that nursery children can never be dropped off in the car park zone and they cannot use the walking bus system run by the school.
- Council must look at the traffic flow in the area, between 0830 and 0915 all roads are congested.
- Parking on Grange Rd, York Rd and Brick Kiln Row should be residents only for certain hours of the day making parents walk their children to school
- A local resident had their car damaged by a parent who drove off (this was mentioned in the schools April newsletter)
- Lack of response from the head teacher on damage to cars causes one to ask about the amount of interest and concern the head has in local residents and neighbours
- Pick up and drop off options at lunchtime for the nursery so traffic burden will increase throughout the day
- No allocated parking for the new nursery staff
- If approved the effective first year increase jumps from 30 to 52 pupils
- There is no reason to suppose that the proportion of new pupils with siblings at the school will be greater than it is for existing pupils.
- Residents were assured that part of first development additional parking would be made available at Bowdon Church – Rumours are that Council funding was withdrawn once the school building begun.

- The school and/or the Council have failed to provide any facility for overspill parking and have failed to take any adequate steps to accommodate the high flow of traffic created by the school.
- A planning application was submitted by the school and/or Council as part of the current development of the school to utilise land north of Grange Rd – This permission has lapsed and the over spill parking not provided
- The school have historically objected to plans to redevelop the school playing fields (2001) traffic and travel safety have and remain a long term problem in the area with the school being highly aware of this.

### Other Issues

- No consultation by school with local residents regarding the proposal
- There is no shortage of nursery places in Trafford
- Trafford need to consider the quality of life for residents who are voters and tax payers
- Sufficient space within the school building or use the hockey club
- More buildings on protected open space
- Temporary building will look unsightly (more such buildings may be required in the future)
- The area is becoming more polluted and noisy
- The new school building is an eyesore
- This proposal should have been in the schools expansion application

### **OBSERVATIONS**

#### PRINCIPLE OF DEVELOPMENT

1. This development proposes the use of the existing SIBCAS (mobile classroom) that was approved under planning ref: 82725/FULL/2014. Under that approval the mobile classroom was relocated towards the eastern boundary of the site. The current proposal does not seek to alter the approved location of the mobile classroom but seeks to change its use from operating as the before and after school club to that of facilitating the pre-school nursery use. As part of the determination of the new school building application (82725/FULL/2014) the proposal involved the loss of open space and was considered against Core Strategy Policy R5 which states that “Development which results in an unacceptable loss of quantity of open space, sport or recreation facilities or does not preserve the quality of such facilities will not be permitted.”
2. Also relevant to the previous application determination was advice contained within the National Planning Policy Framework, paragraph 74 which states that existing open space, sports and recreational buildings and land, including playing fields, should not be built on unless: (1) an assessment has been undertaken which has clearly shown the open space, buildings or land to be surplus to requirements; or (2) the loss resulting from the proposed development would be replaced by equivalent or better provision in terms of quantity and quality in a

suitable location; or (3) the development is for alternative sports and recreational provision, the needs for which clearly outweigh the loss. The approved school application was considered to result in significantly better provision regarding sporting facilities. This current proposal does not alter the approved layout, including the mobile classroom but as stated seeks to alter the approved use of said building.

3. The use of the mobile classroom for use as a pre-school nursery is considered an acceptable use especially within the context of a primary school site; the proposed use complements the wider use of the site. The operation of the nursery will be through a private operator, which however does not alter the principle of the use of this building within the wider school site as being an appropriate use. The pre-school nursery is relocating from a nearby church hall (St Luke's Bowdon Vale) and is therefore not a new business being created.

## RESIDENTIAL AMENITY

4. The nearest residential properties to the mobile classroom to be utilised as part of this development are Woodstock and Lynwood on York Drive and 6 Theobald Rd all of which share their rear boundary with the eastern boundary of the wider school site.
5. Boundary treatment along the eastern side of the school site and in particular with Woodstock, Lynwood and 6 Theobald Rd consists of typical residential fencing with mature trees and hedgerow predominantly along the neighbours side of the boundary as well as a small cluster of trees around the pond located to the rear of 6 Theobald Rd on the school side of the boundary. A concrete post and mesh fence is located along the boundary of the site with Woodstock The natural screen along the eastern boundary of the site offers little opportunity for views in or out of the application site.
6. The mobile classroom will retain a distance of approximately 7.5m at the nearest point to the eastern boundary of the school site.
7. An external play area will be located to the western and southern side of the mobile classroom for exclusive external play by nursery children, the mobile classroom building will screen a significant section of this play area from the eastern school boundary with residential properties, 2x storage sheds will also act as a screen. A grassed area for use by the school nursery will be located to the south side of the main play area
8. The activities associated with the pre-school nursery do not differ from the wider activities within the school site, particularly with regards external play by children and the proposed development is not considered to result in any disamenity to residents regarding activities within the nursery area.

## HIGHWAYS

9. The proposed nursery will not provide any parking for parents. The applicant has indicated that there will be 3.5 members of staff and only parking provision for the staff is catered for within the new car-park as part of the wider school redevelopment. The applicant has provided details regarding the make up of the new intake for September 2015. 21 applications have been received; 14 of the children have a sibling or a parent teacher working at the school (not specified how many are teachers/members of staff). From the 7 children without a sibling or parent working at the school, 3 have indicated that they will drive, 2 of these will be using before and after school clubs. The applicant has concluded that only one extra car at school opening and closing times. Advice within the Councils parking standards indicates that drop-off parking spaces to be determined on a case by case basis.
10. Children for the nursery will have to be accompanied to the nursery building by the parent. The drop off zone for the new school is not designed as a car-park, but for parents to pull in and drop their children off at the school entrance and then leave. Although the applicant has provided a breakdown of this year's intake in an attempt to illustrate that there will be a high number of children with siblings at the school, this does not ultimately change anything. Parents with nursery age children and school age children will still have to park to accompany the nursery child to the nursery building, they cannot do this in the school drop off zone, and the only alternative is on-street parking. Residents from the surrounding streets have been very vocal in their objection to the new school redevelopment but also the proposed nursery building having experienced onstreet parking and congestion.

## DEVELOPER CONTRIBUTIONS

### Community Infrastructure Levy (CIL)

11. Educational development is not CIL liable; therefore the proposed development does not require any CIL contributions.

## CONCLUSION

12. It is acknowledged that the proposed development would have the wider public benefit of improving educational facilities at the site. However the issue of on-street parking and congestion in the immediate area is well documented, this proposal offers no on-site parking provision for parents and it will therefore add to parking on nearby residential streets, a problem that has been reported by residents in high numbers.

**RECOMMENDATION: Refuse, for the following reason:-**

- (i) The proposed development would result in an increase in parking demand which cannot be provided for adequately on site. As such the development would result in increased levels of on-street parking in the vicinity to the detriment of the amenities of the area and its residents and the detriment of highway safety and convenience. As such it would be contrary to Policies L4 and L7 of the Trafford Core Strategy and the Council's approved planning Guidelines for Day Nurseries and Play groups.

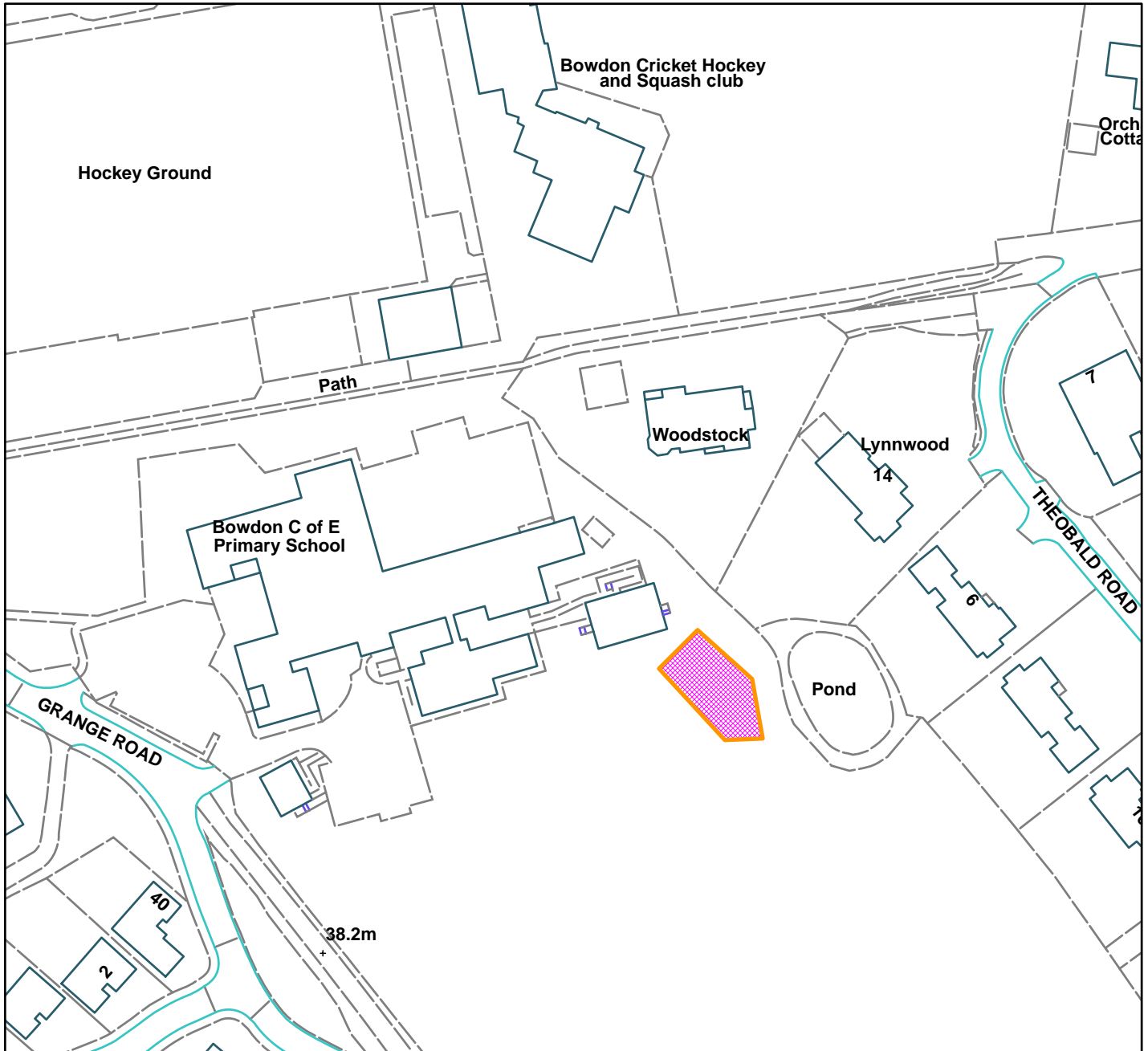
CM

---





Bowdon C of E Primary School, Grange Road, Bowdon (site hatched on plan)



Scale: 1:1,250

Reproduced from the Ordnance Survey map with permission of the Controller of Her Majesty's Stationery Office © Crown Copyright 2012.

Unauthorised reproduction infringes Crown Copyright and may lead to prosecution or civil proceedings.

Organisation	Trafford Council
Department	Planning Service
Comments	Committee date 13/08/2015
Date	03/08/2015
MSA Number	100023172 (2012)

**DEMOLITION OF EXISTING OLD TRAFFORD LODGE; REAR SECTION OF 'A'-STAND; THREE-STOREY 'DEVELOPMENT HOUSE' AND THE FORMER TURNSTILE BLOCK. ERECTION OF A REPLACEMENT 150 BEDROOM HOTEL WITH COFFEE SHOP; NEW TICKET OFFICE BUILDING; 'POP-UP' BAR FACILITY AND OTHER EXTERNAL WORKS, INCLUDING ALTERATIONS TO THE REAR OF 'A'-STAND.**

Lancashire County Cricket Club, Brian Statham Way, Stretford, M16 0PX

**APPLICANT:** Lancashire County Cricket Club.

**AGENT:** Deloitte LLP

**RECOMMENDATION: MINDED TO GRANT, SUBJECT TO A LEGAL AGREEMENT**

---

## **SITE**

'Emirates Old Trafford' is the home of Lancashire County Cricket Club (LCCC). It comprises of a stadium, offices and cricket school that sit within a large site, much of which is covered by surface car parking. The Altrincham – Manchester Metrolink line defines the southern perimeter to the site, whilst Brian Statham Way and Talbot Road form the eastern and northern boundaries respectively.

The stadium also provides events facilities and a 68-bed hotel, known as the 'Old Trafford Lodge', to generate income for the Cricket Club throughout the year. It is this latter building, along with the area immediately surrounding it, which is subject to the redevelopment work proposed within this application.

The Old Trafford Lodge was constructed in 1999 and comprises of a three-storey building that partially rests above an area of terraced seating known as 'A-Stand'. Adjacent to this, and situated immediately at the back of the Brian Statham Way footpath, is another three-storey building known as 'Development House.' This currently accommodates the Club's ticket office and club shop, along with some office floor-space also.

The cricket ground represents the focal point of the 'LCCC Quarter', a Strategic Location as identified within Policy SL3 of the Trafford Core Strategy.

## **PROPOSAL**

Since 2009 Emirates Old Trafford has been significantly redeveloped, with the principal aim of securing more international cricket matches at the ground and to cement its reputation as one of the country's leading cricket venues. An additional aim has been to generate year round funding streams for the Club, for example

through the events facilities provided within The Point. In September 2010 outline consent was granted as part of a wider application (ref: 74393/FULL/2009) to improve and extend the existing Lodge hotel, which has become dated, to bring its total number of bedrooms up to 150. Other aspects of this permission have been built-out, although the hotel element was never implemented and the time limit to submit a reserved matters application for it has now expired.

This application represents the next phase of the redevelopment works at Emirates Old Trafford and seeks Full consent to erect a new 150-bedroom, 4\* hotel that would be operated by Hilton Garden Inn. The development would be sited in-between the eastern side of the Pavilion building and the floodlighting column next to Brian Statham Way; thus it would necessitate the demolition of the existing Old Trafford Lodge hotel, the Development House and part of the 'A-Stand' terrace also.

The Cricket Club have also proposed to demolish the existing turnstile block fronting Brian Statham Way and replace this with a new ticket office building.

The form and external appearance of the new hotel would be designed to closely reflect that of 'The Point' (the events building located on the opposite side of the Pavilion) to create a degree of symmetry to the northern end of the stadium. In addition to the reception and back-of-house facilities associated with the hotel, the ground-floor of the proposal would also accommodate a separate coffee shop, whilst a fitness suite for hotel guests would be situated on the first-floor.

The development would be directly connected to the adjacent Pavilion building via an enclosed, glazed walkway at third-floor level.

Those hotel rooms facing onto the pitch would have floor-to-ceiling and wall-to-wall glazing on their southern elevations, with access out onto a balcony, to allow guests to watch the cricket when a match is in progress. A communal terrace area has been shown at first-floor level to provide outdoor hospitality space on event days.

## **DEVELOPMENT PLAN**

The Development Plan in Trafford Comprises:

- The **Trafford Core Strategy**, adopted 25th January 2012; The Trafford Core Strategy is the first of Trafford's Local Development Framework (LDF) development plan documents to be adopted by the Council; it partially supersedes the Revised Trafford Unitary Development Plan (UDP), see Appendix 5 of the Core Strategy.
- The **Revised Trafford Unitary Development Plan** (UDP), adopted 19th June 2006; The majority of the policies contained in the Revised Trafford UDP were saved in either September 2007 or December 2008, in accordance with the Planning and Compulsory Purchase Act 2004 until such time that they are superseded by policies within the (LDF). Appendix 5 of the Trafford Core Strategy provides details as to how the Revised UDP is being replaced by Trafford LDF; and
- The **Greater Manchester Joint Waste Plan**, adopted 01 April 2012. On 25th January 2012 the Council resolved to adopt and bring into force the GM Joint

Waste Plan on 1 April 2012. The GM Joint Waste Plan therefore now forms part of the Development Plan in Trafford and will be used alongside district-specific planning documents for the purpose of determining planning applications.

- The **Greater Manchester Joint Minerals Plan**, adopted 26<sup>th</sup> April 2012. On the 13th March 2013, the Council resolved that the Minerals Plan, together with consequential changes to the Trafford Policies Map, be adopted and it came into force on the 26th April 2013. The GM Joint Minerals Plan therefore now forms part of the Development Plan in Trafford and will be used alongside district-specific planning documents for the purpose of determining planning applications.

## **PRINCIPAL CORE STRATEGY POLICIES/PROPOSALS**

SL3 – Lancashire County Cricket Club Quarter

L4 – Sustainable Transport and Accessibility

L7 – Design

L8 – Planning Obligations

## **PROPOSALS MAP NOTATION**

The application site falls within the LCCC Quarter, a Strategic Location as identified within the Core Strategy.

## **NATIONAL PLANNING POLICY FRAMEWORK**

The DCLG published the National Planning Policy Framework (NPPF) on 27 March 2012. The NPPF sets out the Government's planning policies for England and how these are expected to be applied. With immediate effect the NPPF replaces 44 documents including Planning Policy Statements; Planning Policy Guidance; Minerals Policy Statements; Minerals Policy Guidance; Circular 05/2005: Planning Obligations; and various letters to Chief Planning Officers. The NPPF will be referred to as appropriate in the report.

## **RELEVANT PLANNING HISTORY**

74393/FULL/2009 - Part Full/part Outline Planning Application for redevelopment of Old Trafford Cricket Ground and erection of food superstore. Full consent sought for the erection of a (Class A1) food superstore (measuring 15,500 sqm gross internal area) incorporating car parking plus associated petrol filling station, landscaping and infrastructure; creation of pedestrian link between Talbot Road and Chester Road; demolition and replacement of existing Old Trafford Cricket Ground stands and other associated buildings/structures to create a new cricket stadium (Class D2) with new media players and education building, extension to existing cricket school, reconfigured and extended members pavilion, spectator seating, hospitality and ancillary facilities including food and non-food retail units, replay/scoreboard screens, sightscreens, 6 no. 60m high floodlighting columns and other associated cricket ground equipment. Outline consent sought for extension to Trafford Lodge Hotel (Class C1) including the creation of a maximum of 82 no. additional hotel rooms, a new fitness suite and a brasserie with details sought for means of access and layout

with all other matters reserved for subsequent consideration. – Approved with Conditions, 29<sup>th</sup> September 2010

H/70373 - Demolition of existing County, Jubilee and Tyldesley Suites and associated area of terraced seating and development of a new grandstand comprising a hospitality and events venue and associated section of new terraced seating (with a total capacity of approximately 2,250). Associated alterations to access road layout – Approved with Conditions, 25<sup>th</sup> February 2009.

### **APPLICANT'S SUBMISSION**

The applicant has submitted a Design and Access Statement; Ecological Report; Crime Impact Statement; Planning Policy Statement; Noise Impact Assessment; Transport Statement and Drainage Strategy as part of their application. The information provided within these documents is discussed, where relevant, within the Observations section of this report.

### **CONSULTATIONS**

**LHA:** No objections; standard conditions recommended.

#### **Pollution & Licensing:**

Contaminated Land: No objections; Standard condition recommended.

Nuisance: No objections, subject to suitable noise attenuation measures being incorporated and a standard condition relating to noise from external plant being added to any permission. Amendments to construction hours have been requested.

**GMEU:** No objections

**GMP: Design for Security:** No objections; standard condition recommended.

**Drainage:** Any comments received will be included within the Additional Information Report.

### **REPRESENTATIONS**

Kate Green MP has submitted a letter which states that she has been contacted by a number of constituents regarding traffic problems on Great Stone Road. They are concerned that the LCCC Hotel proposal will further aggravate this traffic issue.

### **OBSERVATIONS**

#### PRINCIPLE OF DEVELOPMENT

1. Paragraph 24 of the NPPF states that Local Planning Authorities should apply a sequential test to planning applications for main town centre uses that are not in an existing centre and are not in accordance with an up-to-date Local Plan. Hotels and retail development are identified within Annex 2: Glossary of the NPPF as being main town centre uses. Emirates Old Trafford does not sit within an identified town centre however the applicants have sought to demonstrate that the

proposals are in accordance with Trafford's Local Plan and as such do not need to be supported by a sequential test.

2. The application site sits within the Lancashire County Cricket Club Quarter, a strategic location identified within Policy SL3 of the Trafford Core Strategy. This policy supports a major mixed-use development in this location, to provide a high quality experience for visitors (SL3.1). This would be centred around a redeveloped LCCC sports stadium with ancillary sports and leisure facilities (SL3.2). Also of note within the Core Strategy is Old Trafford Place Objective OTO11, which seeks to maximise the potential of Lancashire County Cricket Club as a visitor attraction and its potential to lead regeneration in the area.
3. The Planning Policy Statement submitted with the application states that the hotel proposal is compliant with the policies listed above as it will create a leisure facility that is well designed; provides facilities that are of a higher quality than existing; and that will encourage visitors to the cricket ground to stay overnight in Old Trafford, thus increasing spending in the local area. They state that the approval of hybrid application 74393/FULL/2010 for an improved and extended Old Trafford Lodge shows that this policy is supportive of such a development and has been designed to anticipate a development such as that proposed under the current application.
4. The hotel proposal is considered to align with the aspirations of Policy SL3 as it will clearly support the stadium facility. The ancillary café and leisure elements on the lower floors of the new building are an integral part of this also. As such there is no requirement for a sequential test to be submitted in relation to the main town centre uses proposed within this application as they are considered to be compliant with Trafford Council's up-to-date Local Plan.
5. Policy SL3 also requires development in this location to make a financial contribution towards the provision of a strategic processional route along Brian Statham Way and Warwick Road, in order to be acceptable in planning terms. The processional route will be suitable for a variety of users with a high quality public realm and green infrastructure to enhance visitor experience and link existing and future public transport improvements. The route would link up with a matching set of public realm improvements proposed within the Wharfside area of Old Trafford/Trafford Park, which is also designated as a Strategic Location within the Core Strategy (SL2).
6. The Cricket Club have agreed to pay a contribution of £44,800 to be used specifically for schemes to design and deliver the Processional Route referenced in Policy SL3. This contribution would be secured through a s106 agreement and is in addition to any CIL charges associated with the development.
7. Overall the principle of developing a hotel with associated retail and leisure facilities on this particular site is considered to be acceptable and in compliance with Paragraph 24 of the NPPF and Policies W1 and SL3 of the Trafford Core Strategy. A further assessment shall now be made against the various tests identified within Policies L4 (Sustainable Transport & Accessibility) and L7 (Design) of the Core Strategy.

## LAYOUT, SCALE, DESIGN AND STREETSCENE

8. The Design and Access Statement submitted with the application reports that the proposed hotel development has been designed to balance out the massing of The Point and Pavilion buildings that make up the northern end of the stadium and to continue the 'arena' experience created by these existing redevelopment works. The upper and lower levels of the red frame that wraps around the top three floors of the hotel, along with the rake on its gable walls, are designed to replicate those expressed by The Point. This results in a maximum development height of 19.6m. Similarly, existing features such as rounded staircase cores and an enclosed bridge to the Pavilion have also been included as part of the design of this proposal. The proposed palette of materials for the new building, which include red and grey aluminium panels, curtain-wall glazing, and glass reinforced concrete, has been selected to match those which can be found on the other new buildings that have recently been completed at Emirates Old Trafford.
9. The pitch-facing elevation responds well to the existing structures that sit at the northern end of the stadium, in terms of both its massing and external design. In addition to the features described above, it is considered that the projecting glazed balconies and vertical 'crank' at the mid-point of the building (which allows the hotel to bend around the curve of the pitch) provide important layers of relief and articulation.
10. The massing of the Talbot Road/car park elevation has been adequately broken up and articulated through the use of different (coloured) materials, recessed glazing and a projecting red frame around the top three storeys. The structural concrete frame provides a two-storey plinth for the development that extends around to Brian Statham Way to create shop fronts for the commercial units.
11. The new ticket office is of a similar scale and design to the existing turnstile block that it is set to replace. As such it will have an acceptable impact on the Brian Statham Way streetscene.
12. Overall this is considered to be a development of good design quality that has the potential to enhance the appearance and reputation of Emirates Old Trafford as a leading international cricket venue.
13. The development proposals do not include any new tree planting works, as there is a need to keep the area around the hotel free for servicing, parking and spectator circulation. However the financial contribution for the Processional Route will, in-time, help to secure a series of public realm works that include green infrastructure improvements and which will enhance the setting of the LCCC site. Notwithstanding this planters and shrubs positioned directly in front of the Talbot Road elevation to the hotel will provide some immediate visual impact.

## HERITAGE

14. The application site falls within the setting of the Grade II Listed Trafford Town Hall, which stands on the opposite side of Talbot Road. The building was

designed by Messers Bradshaw Gass and Hope and opened for use in 1933. Its southern elevation, which faces the cricket ground, is considered to be the façade of most architectural significance as it includes several neo-classical features, the main entrance portal and a prominent clock tower. The applicant's submission states that the proposed redevelopment of the Old Trafford Lodge Hotel will greatly improve the visual quality of the site by completing the symmetry of the ground and creating an architectural unity and consistency. It goes on to report that although the design of the new structure will be highly distinctive and eye-catching, it will be positioned sufficiently far away from the listed town hall so as to avoid distracting from it. Reference is also made to the mature tree planting that exists along the boundaries to both the town hall and LCCC sites.

15. The proposed hotel will have a greater presence on its surroundings, including the listed town hall, than that held by the existing lodge building as it provides three more floors of accommodation and includes elements of distinctive red cladding to the car-park elevation. However, as the applicants have indicated within their submission, the design of the scheme is considered to represent a significant improvement over the existing Lodge and its proposed form and palette of materials will help it assimilate with the existing buildings at Emirates Old Trafford. Therefore the impact on the immediate setting of the town hall is considered to be limited and acceptable.
16. The Town Hall was built as a municipal building to serve the local community. It is a landmark in the local area and its clock tower can be seen from the 'Altrincham bound' platform of the Old Trafford Metrolink Station, an important historic view that should be taken into consideration. It is considered that the majority of views of the clock tower from the above platform will remain unobscured as a result of the proposed development and as such its impact on the listed building is acceptable in this particular respect.
17. A turnstile block, situated adjacent to Brian Statham Way and dating from the 1930's, is set to be demolished to free up room for a new ticket office. Whilst relatively simple in its design, the building does include some pleasant art-deco frames around the turnstile doors, along with patterned brickwork on its street-facing façade. Additionally, it represents one of the oldest surviving structures within the Cricket Ground, along with what remains of the 1895 Pavilion. For this reason the turnstiles can be considered as a non-designated heritage asset.
18. Paragraph 135 of the NPPF requires Local Authorities to take a balanced judgement when considering applications affecting non-designated heritage assets. The Cricket Club have indicated that they require separated ticket office facilities, accessed from Brian Statham Way, so that spectators can purchase and collect tickets outside of the ground and away from the hotel on event days. The site of the existing turnstile block has been identified as the optimum location for this building. The Club state that they have given consideration to converting and extending the existing turnstiles but have opted not to pursue this approach as the level of works required would necessitate the removal of those aspects of the block that are of most significance. The reasoning behind the siting of the new ticket office is acknowledged and whilst the loss of the historic turnstile block is regrettable, it is considered that the resulting benefits to Trafford from further



improvements to an internationally renowned cricket ground are sufficient to outweigh this harm. As such this aspect of the development is considered to comply with the NPPF and Policy R1 (Historic Environment) of the Trafford Core Strategy.

## RESIDENTIAL AMENITY

19. The closest residential properties (apartments) are located approximately 200m away from the site of the proposed development. It is considered that the provision of a further 82 hotel bedrooms at LCCC will not directly generate an additional level of activity, for example through comings and goings, that would equate to a demonstrable level of harm to amenity through noise disruption.
20. The Noise Impact Assessment submitted with the application has shown that the installation of standard glazing should be sufficient to ensure that the applicable indoor ambient noise criteria can be met for prospective guests staying at the hotel. A condition should be added to any permission stipulating the maximum noise limits that external plant should adhere to.

## ACCESS AND CAR PARKING

21. The Transport Statement (TS) submitted with the application has shown that the site is accessible by a variety of sustainable transport modes. More specifically it is located within 150m of the nearest Metrolink stop (Old Trafford) and 250m of the nearest bus stops along Talbot Road. On road cycle routing, which connects to the wider network, is also available on several of the surrounding highways.
22. The TS also sets out that the net vehicle traffic generation resulting from the hotel development will be modest, with peak hour traffic volumes through adjacent traffic signal controlled junctions within the normally expected daily variation. This is a conclusion that is shared by the LHA.
23. The Council's Car parking standards indicate that the net increase of 82 bedrooms that will be generated by the hotel development would normally require access to a further 82 car parking spaces. The existing cricket ground car park is controlled by barriers at its two entrances (on Talbot Road and Great Stone Road) and provides 650 bays in total, including 150 in the area of the site that would immediately surround the new hotel. The car park caters for a variety of uses at the cricket ground and is also used by contract holders who park at the site and subsequently travel to work on foot.
24. The Cricket Club has presented vehicle entry data from the barrier system, as part of a car park accumulation exercise to show the total number of vehicles that use their car park across an average week. This exercise has shown that at peak usage (13:00h-14:00h), the existing car park experiences an occupancy rate of 53%. When predicted parking demand associated with the proposed hotel development is factored in, the peak occupancy remains at 53%. The negligible difference between the existing and proposed occupancies can be attributed to the fact that the hotel car parking typically takes place overnight, whereas commuter/employee car parking will normally occur during daytime hours. As a

result the LHA are satisfied that the additional car parking generated by the development can be accommodated as part of the existing car parking provision on site.

25. The series of uses proposed at ground-floor level (ticket office, café, club-shop) are all considered to be ancillary to the cricket stadium and hotel uses on the site and as such carry no specific car parking requirements.
26. A swept-path analysis has adequately demonstrated that the new building can be accessed and serviced by large vehicles. A condition requiring the existing Travel Plan for the cricket ground to be updated has been recommended by the LHA, as has one that requires additional cycle parking facilities to be provided within the stadium for staff of the development.

## FINANCIAL CONTRIBUTIONS

### Community Infrastructure Levy (CIL)

27. This proposal is subject to the Community Infrastructure Levy (CIL) and comes under the category of 'hotels' and consequently will be liable to a CIL charge rate of £10 per square metre in line with Trafford's CIL charging schedule and revised SPD1: Planning Obligations (2014).

### S106 agreement

28. As outlined earlier in this report, there is a requirement for the Cricket Club to contribute towards the design and delivery of a series of public realm works that will form a new Processional Route between the Old Trafford Metrolink stop and Manchester United's Old Trafford stadium. LCCC have agreed to pay a contribution of £44,800 towards this scheme, which will be secured as part of a s106 agreement. The development is therefore considered to be in accordance with Policies SL3 and L8 of the Trafford Core Strategy and revised SPD1: Planning Obligations (2014) in this respect.

## CONCLUSION

29. In conclusion, it is considered that the proposed hotel development would make a significant contribution to further enhancing the appearance and reputation of Emirates Old Trafford, which attracts thousands of visitors each year to this Strategic Location within the Borough. The Transport Statement submitted with the application has adequately demonstrated that the site already has sufficient car parking to meet the demands of the development and that the level of trip generation associated with it at peak hours will be limited. The scheme will contribute monies through s106 and CIL that will help to deliver important infrastructure projects, both within the immediate vicinity of the site and the wider Trafford area also. Therefore, on this basis, the development is considered to be compliant with all relevant policies set out within the NPPF and the Trafford Core Strategy.

**RECOMMENDATION: MINDED TO GRANT SUBJECT TO A LEGAL AGREEMENT**  
**and the following conditions:-**

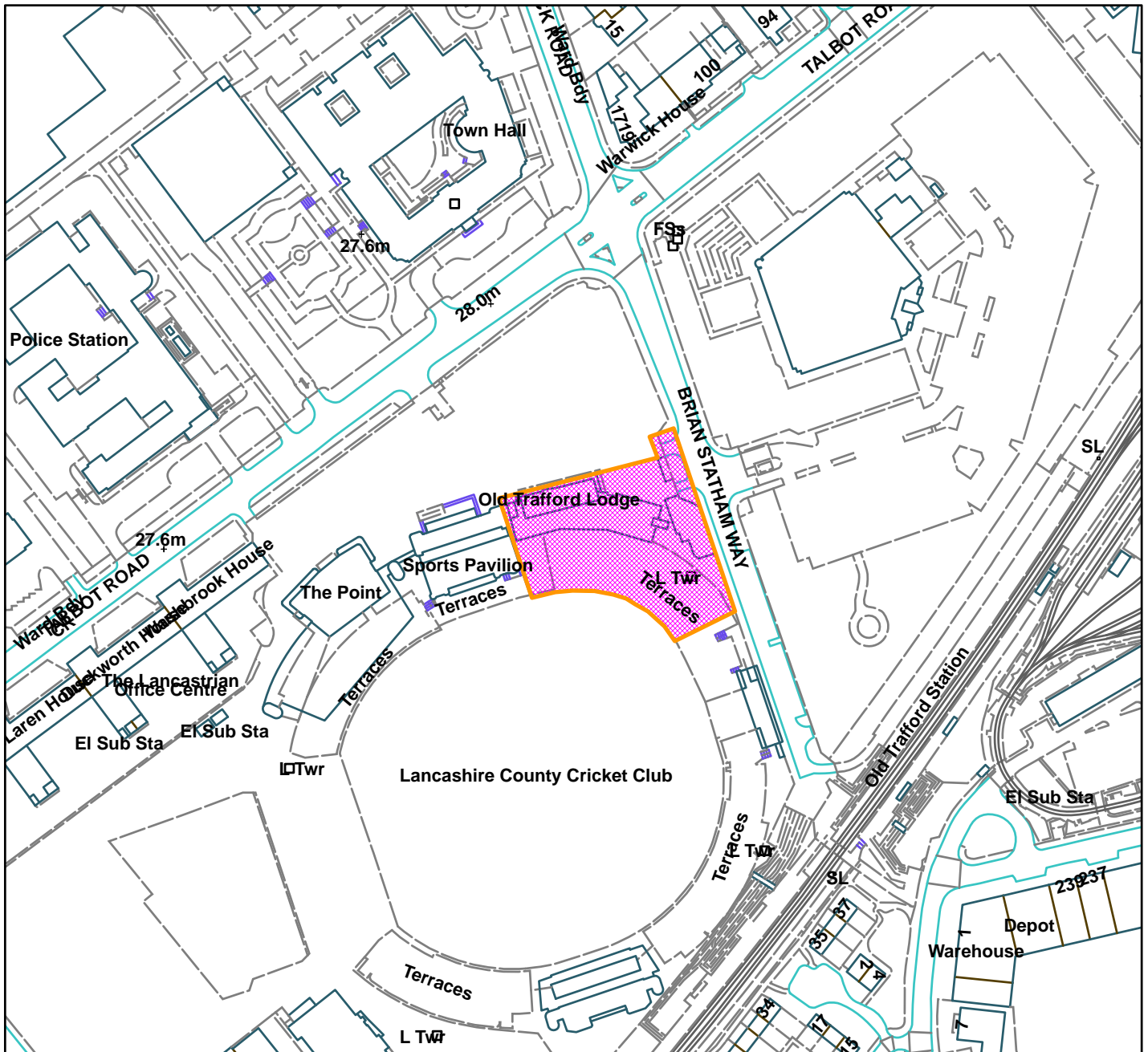
- (I) That the application will propose a satisfactory development for the site upon completion of a legal agreement which will secure a contribution of £44,800 towards the design and delivery of a Processional Route (as defined within Policy SL3 of the Trafford Core Strategy).
- (II) In the circumstances where the Section 106 agreement has not been completed within 3 months of the date of this resolution, the final determination of the application shall be delegated to the Head of Planning Services.
- (III) That upon satisfactory completion of the above legal agreement, planning permission be granted subject to the following conditions: -
1. Standard;
  2. Compliance with all Plans;
  3. External materials;
  4. Hard landscaping scheme;
  5. Drainage, to include discharge storm water at a peak rate that accords with the limits set out within the Manchester City, Salford and Trafford Level 2 SFRA;
  6. Prior to the commencement of any works (demolition), a Construction Management Plan (CMP), to include details regarding working hours, dust and noise suppression, and wheel washing for vehicles leaving the site, shall be submitted to and approved by the LPA.
  7. The combined external plant noise level, when rated in accordance with BS 4142: 2014, and including a character correction of +5dB, should not exceed 69dB LAeq at 3m distance.
  8. A detailed scheme for the alterations to the Brian Statham Way footpath shall be submitted to, and approved by, the LPA;
  9. LCCC Travel Plan to be updated and submitted to the LPA for approval;
  10. Cycle Parking;
  11. Development should proceed in accordance with the approved details set out within the approved Crime Impact Statement;
  12. Contaminated Land;

JK

---



Lancashire County Cricket Club, Brian Statham Way, Old Trafford (site hatched on plan)



**Scale:** 1:2,500

Reproduced from the Ordnance Survey map with permission of the Controller of Her Majesty's Stationery Office © Crown Copyright 2012.

Unauthorised reproduction infringes Crown Copyright and may lead to prosecution or civil proceedings.

Organisation	Trafford Council
Department	Planning Service
Comments	Committee date 13/08/2015
Date	03/08/2015
MSA Number	100023172 (2012)